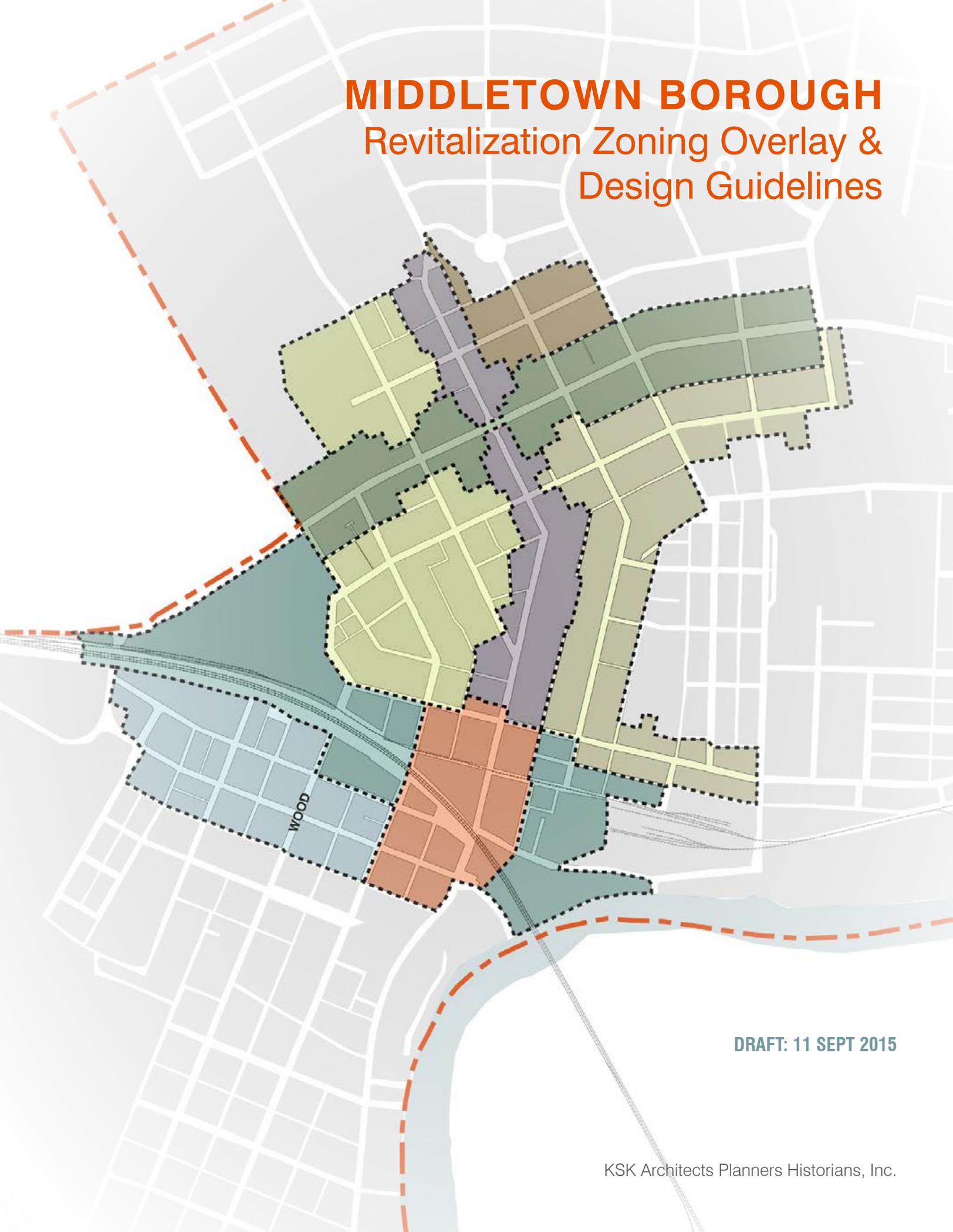


MIDDLETOWN BOROUGH

Revitalization Zoning Overlay & Design Guidelines



DRAFT: 11 SEPT 2015

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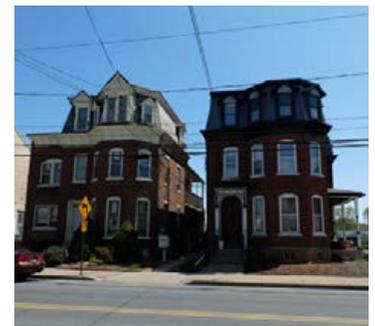
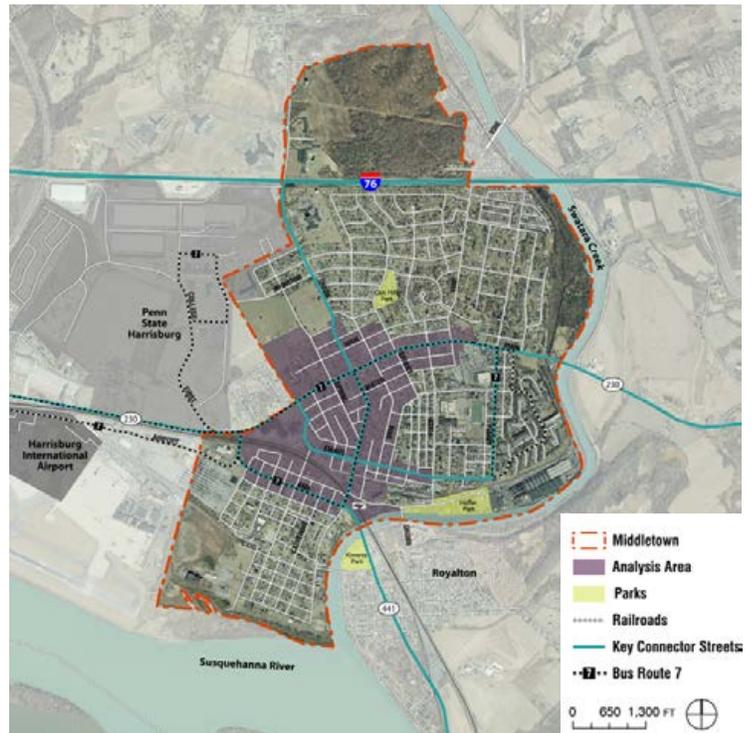
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INTRODUCTION

Background

The oldest town in Dauphin County, the Borough of Middletown has an architecturally distinctive core, with many handsome commercial and residential buildings and a wide variety of architectural styles that create an attractive walkable streetscape. The core of the town center, at the intersection of S. Union Street and E. Emaus Street, extends south along South Union Street to the AMTRAK rail line. The core is surrounded by a traditional urban fabric of 2- and 3-story single, twin, and attached houses built at or close to the street line, typically with rear alleys providing access to parking. The residential urban fabric consists of a rich variety of styles and materials, including brick attached homes with mansard roofs, cornices and ornamental details, and pitched roof porch-front twin and single homes with clapboard siding. However, in some places the traditional historic fabric of the Borough has been compromised in recent years by development that, while consistent with the zoning regulations in place at the time, is inconsistent with, and detracts from, the traditional building fabric of the Borough. At Center Square, at the intersection of Main and Union, the historic original center of the Borough, the setting has been compromised by construction of the auto-oriented, 7-Eleven, which is setback for the street behind parking. Similar auto-oriented developments have compromised the physical fabric of Main Street at other key intersections. In the Borough core, many buildings of architectural interest that contribute to the character of Middletown have been inappropriately altered. In the residential areas, character defining historic architectural details have been removed or covered on many buildings. In addition, public streetscape conditions are often uninviting. While in the past most streets in Middletown were lined with street trees, most blocks now have few trees and little landscaping. Widening of cartways on major streets such as Main Street to accommodate vehicles has resulted in narrow sidewalks and a poor pedestrian environment. Parking lots outside the historic core are typically unscreened and have minimum or no landscaping. Rear alleys providing access to many parcels have been haphazardly developed, with garages, and some accessory dwellings.

MAP 1: Middletown Context Area



Clockwise from the top: town center at Emaus and Union Streets; historic residential building fabric; haphazard rear lane development; unscreened parking lot

Middletown has several positive attributes that make it an excellent candidate for new investment. The 3,500 student Penn State Harrisburg Campus to the west is a less than 15-minute walk from the center of the Borough, and there is a shared interest between the University and the Borough to make Middletown to make the two areas better connected and integrated. The Borough is well served by its transportation network; it is served by the AMTRAK Harrisburg Rail Line, the Harrisburg International Airport is nearby, and it has good access to the regional highway network. The AMTRAK Station is proposed to be relocated to a cleared site in the vicinity of the intersection of Main and Ann Street, west of the town center close to the Penn State Campus and airport; the relocation would include the extension of Emaus Street from the current town center to the new station. This site, and adjacent deteriorating shopping center parcel offer the potential for transit-oriented, pedestrian-friendly development that can act as a link between the camps and town center. Because of its locational attributes, the Borough is anticipating new investment that will include renovation of existing buildings for existing and new uses, new infill construction, and new mixed-use development and an increased residential population.

The goal of the Borough is to build on the positive features of the Borough core, and encourage reinvestment that respects the existing historic growth patterns and architectural character of the building fabric, while creating an attractive thriving economically successful pedestrian-friendly community. An attractive high-quality coherent building fabric and streetscape can help create a unique identity for the Borough, increase property values, and help marketing the Borough to new residents, businesses and developers. An attractive, high quality, coherent building fabric and streetscape can help create a unique identity for the Borough, increase property values, and help market the Borough to new residents, businesses, and developers. Zoning regulations need to support desired future patterns of development, and respect the existing building fabric and enhance the image of the Borough.



Penn State Harrisburg Campus

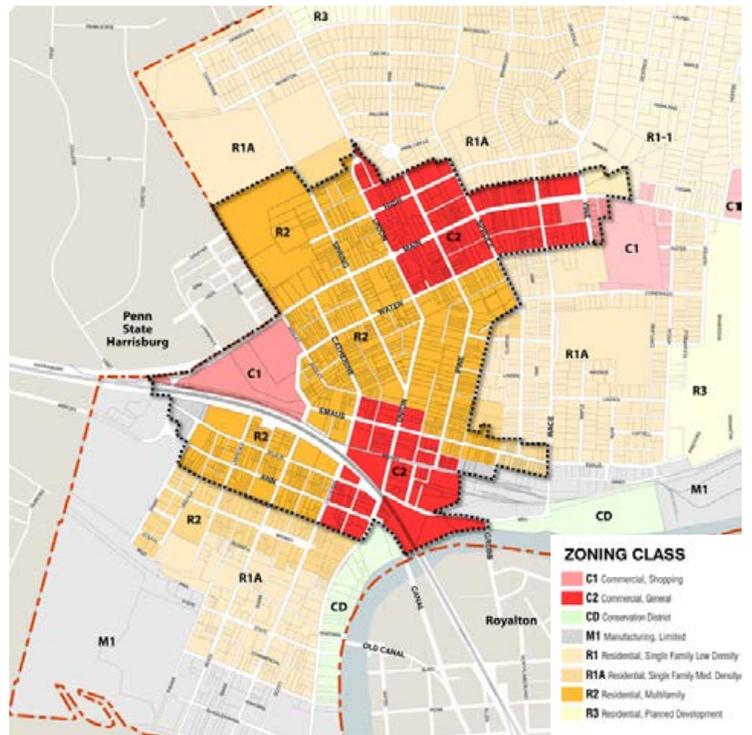


Proposed AMTRAK station relocation site

2013 Zoning Ordinance

Zoning should encourage appropriate types of development, and rehabilitations that are inconsistent with goals for revitalization that contribute to an attractive, economically healthy community and development of the Borough, and discourage inappropriate types of development. Until passage of the new Borough zoning ordinance in 2013, there had been a major disconnect between the form and character of the historic building fabric of the Borough and the uses and physical form of buildings that resulted from the zoning use and dimensional regulations. The area of the Borough subject to the zoning overlay and design guidelines is zoned C1 and C2, Commercial and R2, Residential. The central core along South Union Street from Emaus Street to the Amtrak Station and Ann Street is zoned C-2. The building fabric in the core is characterized by 1- to 4-story attached mixed-use buildings, built close to or at the street line, on relatively small lots, with rear lane access for parking, as well as on-street parking. However, the previous C-2 zoning regulations called for a front yard depth of 60 feet, conflicting with the character of the Borough core (although the previous code had also included a provision that the front yard depths could be reduced to the average of the adjacent buildings). Mixed-use and residential buildings and retail stores, shops and hotels, the traditional uses of a town center, were not permitted under the C-2 zoning regulations. Dwellings were specifically prohibited in the C2 zone. One off-street parking space was required for every 200 square feet of commercial use, a provision that would be impractical to meet within the context of the historic building fabric. Most existing buildings in the town center did not conform to the previous zoning ordinance in terms of dimensional and use regulations, or parking requirements. Similarly, the C-2 zoned commercial area along E. Main Street includes many attractive traditional, predominantly 2-story, detached, semi-detached and attached buildings, constructed at the street line, or setback a few feet to accommodate a front porch. Many buildings are residences. The historic building fabric within the Main Street commercial area could not have been built under the previous zoning regulations. Although the code included a provision for reduced setbacks, if abutting buildings had smaller setbacks, the required 60' setback resulted in developments that conflicted with the character of Main Street.

MAP 2: Current Middletown Zoning



Clockwise from the top: C2 zone on Union Street; C2 zone on Main Street; building in a C2 zone with parking in front

There was no maximum setback, resulting in buildings setback behind parking, and loss of historic building fabric at key intersections. Residential uses were not permitted in C2 under the previous code.

The previous highway-oriented C1 zoning regulations applicable to the Middletown Car Works/Middletown Shopping Center development site did not encourage the type of mixed-use development appropriate for this key development parcel, strategically located between the college and the Borough historic commercial core. Permitted uses included automobile service stations, and automobile sales and repair garages. Maximum allowed building height was only 35 feet, except for non-profit apartment housing for the elderly, which could be built to a height of 135 feet.

Most of the historic residential blocks immediately abutting the commercial core west and north of downtown are zoned R-2, multi-family. The R-2 district permits a range of single-family, and two-family detached and semi-detached dwellings, single-family attached dwellings, apartments and professional offices, consistent with existing building fabric. However, the previous R-2 zoning regulations required a minimum front yard depth of 30 feet, inconsistent with the traditional building fabric of Middletown (this could be reduced to the average of adjacent properties, but there was no maximum setback). Multi-family apartment buildings required a minimum lot width of 100 feet. Most residential buildings within the historic residential blocks adjacent the commercial core are built close to the lot line, with small setbacks.

No new buildings could be readily constructed under the previous zoning regulations without disrupting the attractive building fabric of the Borough. The new Borough zoning ordinance, adopted in 2013 addressed many, but not all of the deficiencies in the previous code. It also does not provide positive guidance on the type, character, and appearance of new development, and rehabilitation, so that it makes a positive contribution to the revitalization of the Borough commercial core and adjacent historic residential areas. The new code does not prevent or discourage inappropriate demolition or alterations of existing buildings that contribute to the character of Middletown.



Middletown Shopping Center



R2 zoned houses on Spring Street



Large setbacks for multi-family housing

2013 C2 Regulations

While the previous C2 regulations applicable to the commercial core did not permit residential uses, the new regulations now include a statement that an objective of the zone is to “permit a wide range of business types and residential uses that are compatible with the urban character.” However, residential uses are not permitted as-of-right. Only “residential hotels” are permitted as a special exception. The previous requirement for a 60’ front-yard setback has been eliminated, and no front-yard setback is now required. However, there is no “build-to-line” or maximum setback required, to assure that placement of new development is consistent with adjacent buildings. Under the new code, parking is sensibly not required for a change in use in an existing building. A prohibition on parking between the building and street line is now provided, as well as a requirement that parking be placed to the rear of buildings, and accessed from an alley, existing curb cut, or side street, thus encouraging more pedestrian-friendly development. Under the new C2 regulations, parking garages are now permitted as-of-right, and may be located above or below ground. Above-ground garages must be located in the rear yard and be “architecturally compatible with other improvements on the site and immediate area.” ECHO housing (accessory dwelling units) are now permitted in C2 zones by Special Exception. However, there are significant restrictions, including a maximum floor area of 900 sq.ft., a minimum requirement of one additional parking space in addition that required for the principal use, and a requirement for conformance to the same front, side and rear yard setbacks as the principal use. Apartment buildings are still not permitted as-of-right in the C2 zone. Housing for the elderly is permitted by special exception in the C2 zone, but is subject to significant constraints, including a minimum lot area of 40,000 sq.ft., a maximum density of 18 units to the acre, and a minimum front-yard depth of 100 feet.

2013 C1 Regulations

Under the previous code, apartments were a permitted use in the C1 District. Under the new code, all residential uses permitted in any residential district, are permitted with the exception of conversion apartments. However, residential uses are now only permitted as an accessory use, where located above a permitted commercial use, which limits the flexibility of providing a multiple use development, as opposed to a mixed-use development, as the mix of uses must be within the building, not just within the site. Under the previous code, no front yard was required. A minimum 10’ setback is now required. Under the new code, minimum lot size is 2 acres, except for individual retail uses, which is unnecessarily restrictive. There is a 35’ height limit, except for hotels, a height that limits the potential for mixed-use buildings.

2013 R2 Regulations

Under the previous code, apartment buildings were a permitted use in the R2 residential zone, subject to restrictions, including a minimum lot width of 100’, a minimum lot area per dwelling unit of 2,000 square feet, and a maximum floor area ration of 0.4. The minimum front-yard requirement was 30 feet. Under the new code, apartment houses are only permitted as a special exception, subject to the same restrictions. Under the previous code, existing single-family dwellings could be converted to up to 4 dwelling units, as of right with a minimum floor area of 650 sq.ft. each, which encouraged conversion of owner-occupied single-family residency to multi-family rentals, with an adverse impact on well-maintained residential blocks. Under the 2013 Code residential conversions are no longer permitted as of right in the R2 zone. Student housing is now permitted in the R2 zone as-of-right, subject to certain restrictions, including a minimum apartment area of 650 sq.ft., provision of off-street parking, and a lot area of 2,000 sq.ft. per dwelling unit, a minimum lot width of 100 feet, and a total lot coverage ratio of 60% (65% by special exception). ECHO housing accessory units are a permitted use by Special Exception, as in the C2 zone. The previous code required a minimum front-yard depth of 30 feet, which was inconsistent with the R2 neighborhood fabric where most homes are built to the back of the sidewalks, or with typically a small 10’ setback. The required setback has been reduced to 10’, with a provision that this can be reduced if more than 50% of the lots on the block frontage have a smaller front-yard depth. However, there is still no required “build-to” line in the new code with a maximum setback to assure that new construction is consistent with the adjacent building fabric. In some cases, larger setback may be appropriate to respect conditions on the specific block.

Parking

Under the new code, required parking provisions for commercial uses have been reduced. The previous code required an excessive 5 spaces per 1,000 sq.ft. of retail and commercial office space. This has been reduced to a more reasonable 4 spaces per 1,000 sq.ft., and 2-1/2 gross per 1,000 sq.ft. in the C2 zone. However, parking requirements for residential uses have not been reduced, and still require 2 spaces for every single and 2-family dwelling unit, and 1.33 spaces per unit for apartments and student housing. However, in the new code, parking requirements do not apply to buildings in the C2 zone, except for new buildings and additions.

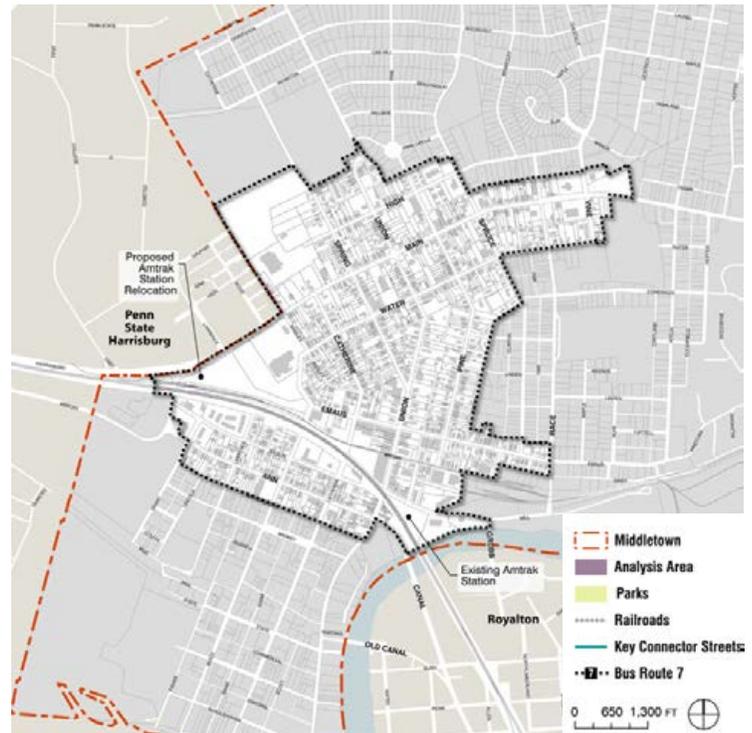
MIDDLETOWN REVITALIZATION ZONING OVERLAY

While the 2013 Code addressed many of the deficiencies in the previous code, it does not provide the detailed design guidance and standards that are needed to make sure that developments and rehabilitations in the Borough core contribute to the revitalization and conservation of the attractive traditional Borough building fabric. A zoning overlay is proposed that will provide additional development guidance and regulations within the Borough core, and “gateways” to the Borough. The proposed boundaries of the Revitalization Zoning Overlay are based on an analysis of the existing building fabric of the Borough.

The parameters for selection of the revitalization overlay boundaries include the following:

- Extent of the traditional historic building fabric of Middletown Borough, with walkable scale streets, small lots and fine-grained buildings built close to the sidewalk, with rear lane vehicular access, and a variety of architectural styles and mix of uses.
- Inclusion of primary “gateway” routes to the Borough, and high visibility intersections that provide visitors first impressions of the Borough.
- Inclusion of key redevelopment opportunity sites that can contribute to revitalization of the Borough core.
- Inclusion of buildings and blocks of historic and architectural significance that contribute to the positive characteristics of the Borough core.
- Inclusion of blocks with some level of deterioration, where revitalization opportunities could be enhanced through the overlay district.

MAP 3: Analysis Area



Character Districts

Within the proposed overlay zone, distinct “character districts” have been defined that have specific defining features and development issues, and require specific regulations and design guidelines to address these issues. The various “character districts” have been defined based on analysis of existing conditions, in the analysis area including:

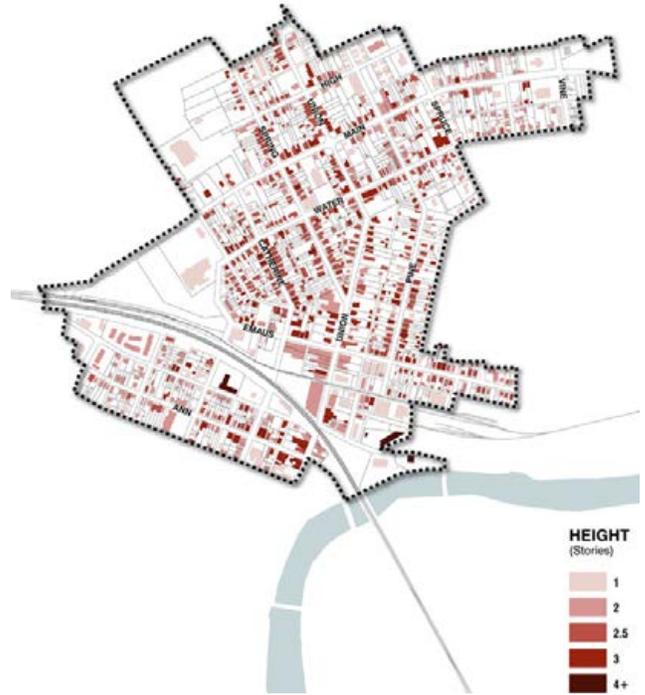
- Gateway corridors,
- Concentrations of buildings of historical and architectural significance
- Analysis of building and lot dimensional variations, including frontage, setbacks, building height, and building typology.
- Land use
- Building conditions
- Streetscape conditions

The series of maps on the following page illustrates the existing conditions that were mapped and analyzed in the analysis area to determine the character districts.

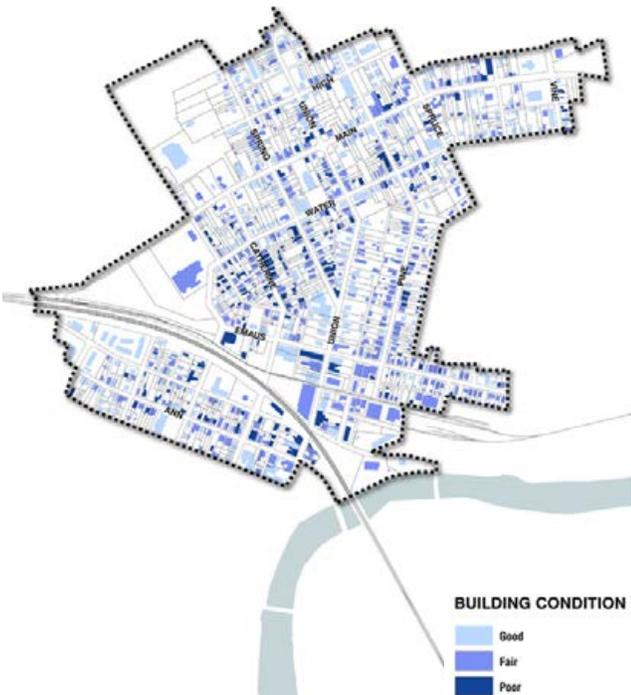
MAP 4: Historical Significance



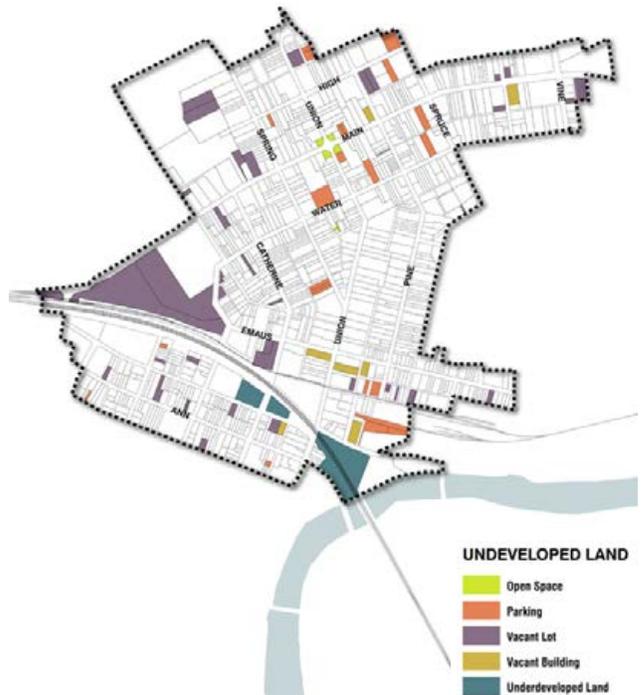
MAP 5: Building Height



MAP 6: Building Condition



MAP 7: Undeveloped Land

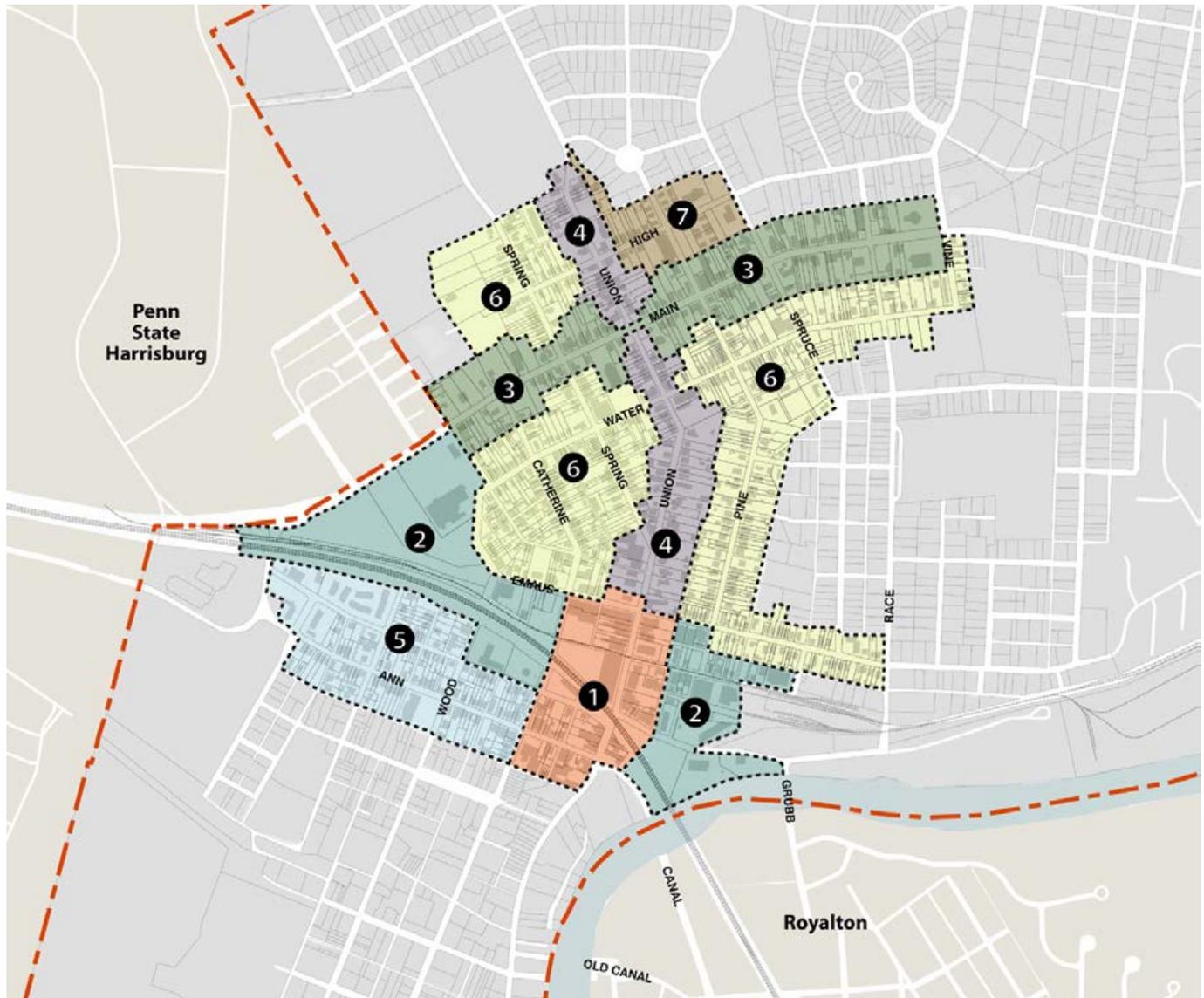


Zoning Overlay Zone Boundaries

The proposed Middletown Revitalization Zoning Overlay Area is shown on Map 8. It generally encompasses the historic core of Middletown, extending from Carmony Avenue, St. Peter's Avenue, and Hoffman Avenue in the north, Vine and Peony in the east, Witherspoon in the south and Main Street and Wood Street in the west. It includes the major "gateways" to the Borough. The proposed overlay area includes a number of different character zones, each with its own attributes. Each character zone will require distinct overlay regulations and design standards to address the issues. Specific district character zones identified include the following:

1. **Middletown Town Center Core**
2. **Expanded Town Center Core**
3. **Main Street Corridor**
4. **North Union Street Corridor**
5. **Ann Street Corridor**
6. **Other R2 residential multi-family zoned areas outside key character districts.**
7. **C2 Commercial zoned area east of Union Street outside key character districts**

MAP 8: Middletown Revitalization Zoning Overlay Area



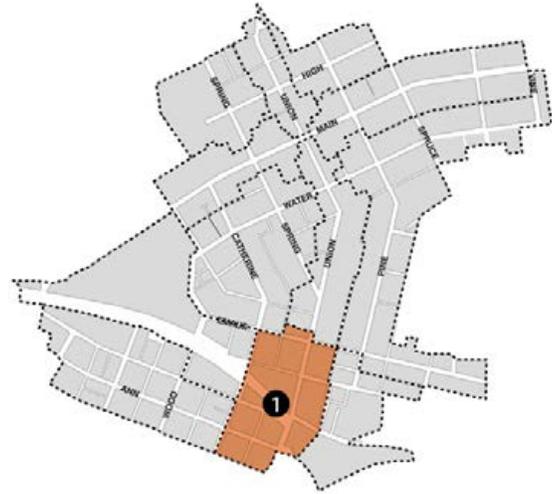
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MIDDLETOWN TOWN CENTER, CORE

Existing Character

The pedestrian-oriented historic retail, entertainment, and administrative core of Middletown is centered on South Union Street, between Emaus Street in the north and Witherspoon Street to the south. It encompasses an area generally bounded by Emaus Street, South Catherine Street, Poplar Street and Witherspoon Street. The building fabric on South Union Street typically consists of 1- to 4-story (25'-50') high buildings built to the street line within a more or less continuous building frontage for the full length of a block. Most buildings are two stories high. Individual building widths vary from 20'-60' and average 25'-30' in width. Buildings along South Union Street have traditionally been mixed-use, with first floor retail storefronts, and residential or commercial office uses above. Buildings are typically of brick masonry construction, with originally fully glazed retail storefronts at first floor level, and punched window openings and bay windows on upper floors. Buildings traditionally incorporated parapets, and or cornices, in conjunction with flat roofs and created an attractive pedestrian-scale. Service and parking is provided from rear lanes, Scott Avenue to the west, and Poplar Street and Astor Avenue on the east.

In recent decades, former storefronts at the south end of Union Street have been enclosed, and replaced by residential occupancies. In other cases, more recent development has turned its back to Union Street. The Karns Grocery Store facade on fronting Union Street is mostly without windows facing the street, with a 160' plus blank brick wall at the south end. Original first floor and upper floor windows of the other Karns buildings have also been blocked, except for the corner entrance, creating an unattractive environment for pedestrians on Union Street. The core retail block of South Union Street between Brown and Emaus still retains most of its original building fabric and glazed storefronts within an attractive pedestrian-scale mix of 1-, 2-, and 3-story, fine-grained buildings, 25'-70' wide, built to the street line. However, many have been poorly renovated over the years. West Emaus Street between South Union and Catherine Street, includes the historic Elks Theatre Building, as well as the Borough administration building, an adaptive reuse of the former Romberger Hosiery Mill, remodeled by the WPA in 1936. Rear service lanes and party areas are typically uninviting, and without landscaping.



Mixed-use development on Union Street



Karns Grocery Store Facade on Union Street



Recently renovated storefront next to un-screened parking on Union Street

Development and Design Issues in Town Center Core

Basic issues in the historic Middletown Town Center core that need to be addressed in the zoning overlay design guidelines in order to create a more pedestrian-friendly environment, include the following:

1. Inappropriate Storefront Modifications

Many storefronts on buildings on South Union Street have been altered in the last 30 years. Many of these alterations have not respected the architectural character of the buildings. Several buildings have been altered with the addition of applied wood-shingled or asphalt-shingled pent eaves. The pent eaves detract from the architectural character of the buildings, and cast shadows on storefront window displays. However, more recently some of these pent eaves have been removed and the original storefronts restored to their original form, significantly enhancing the appearance of the buildings.

2. Blocking of Upper Floor Windows

On several highly-visible buildings fronting South Union Street, upper floors have been blocked, creating blank uninviting facades. These include the former Elks Building at South Union and Emaus Street, and the Karns Grocery Store Building on Union Street.

3. Blank Walls at First Floor Level

Unarticulated blank walls at first floor level on Union Street create an uninviting environment for pedestrians. The Karns Grocery Store has a 160' long unarticulated brick wall at pedestrian level on Union Street north of the rail line.

4. Infilling of Former Storefronts

Former storefronts, such as 208 South Union Street, have been infilled to permit conversion to residential use, without any mitigating architectural treatment that respects the original design, creating an unattractive pedestrian environment. In other cases, such as at 223 South Union Street, former storefront windows have been substantially reduced in size, creating an uninviting facade.

5. Refacing of High-Quality Traditional Building Facade Materials With Poor Quality Modern Materials

On several buildings, original high-quality building facade materials have been covered with low-quality modern residential materials. The building at 203-207 South Union Street has been refaced in low-cost

vinyl siding and upper floor windows reduced in size. Similarly, the metal-faced bay window and upper brick facade at 219 South Union Street and the adjacent building at 223 S. Union Street have been refaced in low-cost vinyl siding.

6. Underdeveloped Sites

The downtown core includes several underdeveloped sites at the periphery of the zone that could offer redevelopment opportunities. These include the Henderson Tarp property at the northwest corner of South Union and Witherspoon streets, and the old Pennsylvania Railroad Freight Station site on Wilson Street, west of South Union.

7. Inappropriate Building Setbacks

Most buildings within the core are built to the street line. The continuity of the urban streetscape is interrupted by buildings setback from the street line, such as 216 South Union Street as required under the old zoning code. While the new zoning code does not require a setback at this location, there is no maximum permitted setback.

8. Unscreened Parking Lots Without Landscaping

The rear alley access parking lots serving the town center core commercial establishments are unattractive, and incorporate minimal or no landscaping. These include the lots on Astor Street and Poplar Street to the rear of the properties on the east side of South Union Street, the Brownstone Café lot abutting Emaus Street and the lots to the rear of the Karns Grocery Store off South Catherine Street, and the lot behind 223 South Union Street. The new zoning code requires a minimum 3' wide "vegetative planting area" around the perimeter of an off-street parking lot but does not require any screening. For commercial uses, the new code requires that new lots be a minimum of 5% of the interior surface area of the lot be devoted to vegetative cover with a mix of trees, shrubs, ground cover, and lawn.

9. Inappropriate Street Tree Species

The existing street trees on South Union Street are not appropriate for a retail environment. Their broad, dense, heavy-leaved character blocks views of storefronts and signage. Street tree species with a more open leafed columnar character that are more appropriate to a retail environment should be planted.

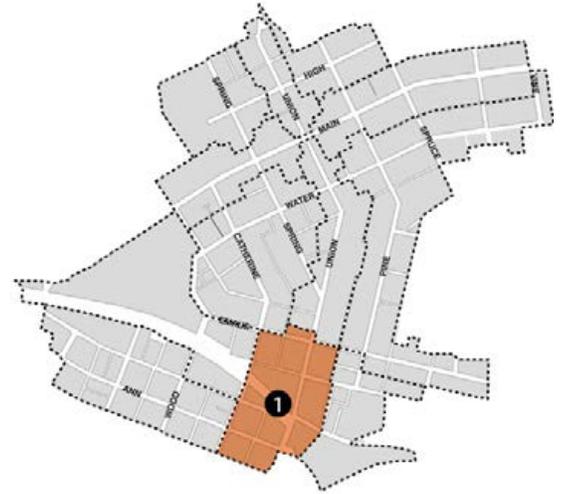
10. Lack of Residential Uses

Upper floors of many buildings within the town center core are not fully utilized and offer the potential for residential use to provide more 24-hour activity in the area.



Development Goals

- Protect, reinforce, and enhance the positive characteristics of the traditional town center core buildings and streetscape, through sensitive rehabilitation and new construction.
- Protect and enhance the mixed-use character of the traditional town center core, with ground floor retail activity along South Union Street, and residential uses on upper floors.
- Encourage mixed-use development of underutilized and vacant sites
- Create attractive pedestrian-friendly streetscapes.
- Reduce and minimize the adverse visual impact of parking lots.



Overlay Regulations and Design Standards and Guidelines

The following Overlay Zoning Regulations and Design Guidelines are proposed to assure that new development and rehabilitation is consistent with the goals.

Uses & Dimensional Standards

The goal of the Town Center Core Zoning Overlay is to encourage higher density, multiple use, pedestrian-oriented rehabilitation of the historic building fabric, as well as appropriate multiple use pedestrian-oriented new construction on underdeveloped sites.

TABLE 1: Zone 1 Permitted Uses and Dimensional Standards

Permitted Uses	Apartments above the first floor are permitted as-of-right	
Dimensional Standards	<i>Front Yard</i>	Maximum front yard setback shall be 10'. Buildings fronting South Union Street between Emaus Street and Witherspoon Street shall be built to the front lot line. Elsewhere front yard setback shall match that of adjacent buildings
	<i>Building Height</i>	Maximum building height for all uses shall be four stories or 50'.

Building Design Standards and Guidelines

Demolition

No building identified as of historical or architectural interest within the overlay zone shall be demolished without review by the Planning Committee, a public hearing, and approval of the Zoning Hearing Board.

Rehabilitation of existing buildings

Repair and rehabilitation of street facing facades of existing buildings or adaptive reuse of buildings identified as of historic or architectural interest within the Overlay Zone shall respect the original character of the building. Original materials shall be retained and rehabilitated where feasible. Inappropriate changes, such as reducing the size of or infilling of window openings, and infilling glazed storefronts shall be prohibited. Inappropriate additions not conforming to the original design intent of the building, such as pent-eaves and fake shutters shall be avoided. Applied facings of low-grade residential quality materials such as vinyl siding shall be prohibited.

New construction

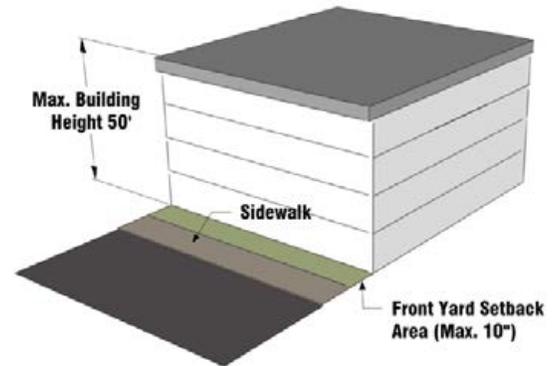
Within the overall massing envelope permitted under the overlay zoning, new construction with the town center core shall respect the character of the historic building fabric. Articulation of the building façade shall be provided to break up the mass of the building to create an attractive pedestrian environment on street facing facades. Street facing facades shall be designed for visual interest at the pedestrian scale. Long undifferentiated street walls shall be avoided.

Vertical articulation of the building shall be provided through changes in plane and/or changes in materials or projecting features such as bays. Vertical articulation shall be provided at a spacing that corresponds to the traditional building width in the Middletown Town Center core, between 25' and 30'. Changes in plane shall be a minimum of 1'-0".

Horizontal articulation of the building shall be provided to emphasize the base, middle, and top of the building through changes in fenestration materials, bays, cornices, parapets and other features to create visual interest at the pedestrian scale. The sidewalk level of the building shall be differentiated through changes in material, plane of facade, amount of fenestration, or other similar means.

Building entrances shall be clearly identified and visible from the street and shall be emphasized through design features such as prominent or recessed doorways and surrounds, entrance canopies, glazed lobby areas, changes in facade plane or materials, etc.

DIAGRAM 1: Front Yard and Building Height



Attractive storefront renovation on Union Street



Building with horizontal and vertical articulation



Clearly identified building entrance



Ground floor windows



Upper floor windows

For buildings with first floor retail and commercial uses, a minimum of 60% of the ground floor facing the primary street sidewalk shall be transparent windows.

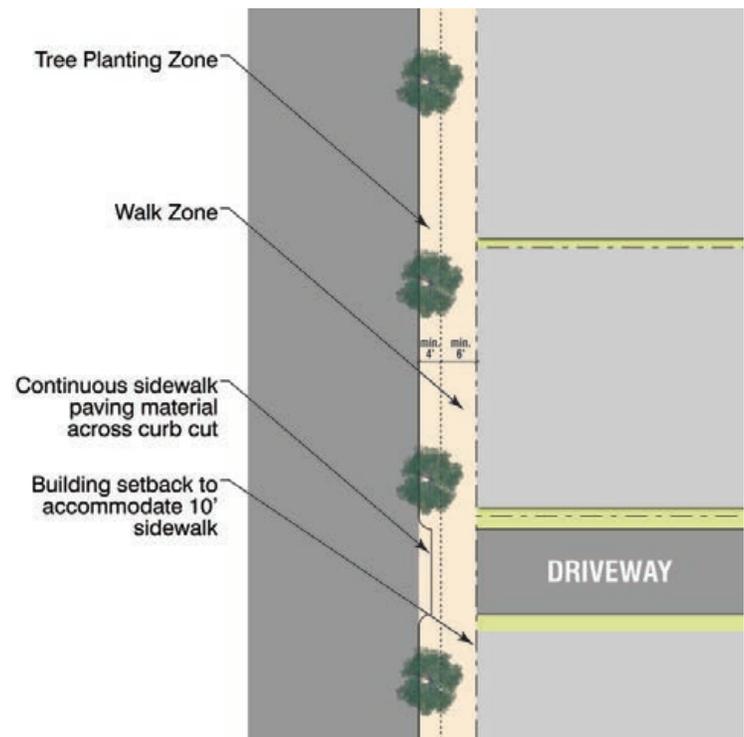
Where blank walls on side streets are unavoidable for programmatic reasons, such walls in excess of 30' long shall receive special treatment to create pedestrian interest. Such treatments may include articulation of the wall surface through changes in materials, textures, or wall planes. Such walls shall not front on primary pedestrian-streets, including South Union Street and Emaus Street.

A minimum of 20% of the upper floor façade shall be transparent glass windows.

Exterior wall materials shall be high-quality materials that are consistent with the traditional building fabric of the Middletown Town Center core and may include brick masonry, tile, stone, cement stucco, metal, and glass. Painted or unpainted plain concrete masonry units, vinyl siding or similar residential quality materials shall not be permitted.

No parking lot driveway access shall be permitted from South Union Street.

DIAGRAM 2: Sidewalk Dimensions



Streets and Site Planning

Sidewalks

To encourage pedestrian traffic, sidewalk paving material shall be continuous across any driveway or curb cut. No curbed driveway access across sidewalks is permitted. Sidewalks shall be a minimum 10' wide, including a minimum 6' walk zone and minimum 4' tree planting zone. New buildings shall be setback sufficient to accommodate the 10' wide sidewalk, a portion of which may extend into the lot depth.

Street Trees

Street trees within the town center core shall be retail-friendly street trees, columnar in form, with an open leafed character, to permit views of storefronts and buildings. Acceptable tree species include Princeton Sentry Ginko, Imperial Honey Locust, and Fastigate English Oak. Street trees shall be planted at an average of every 30'.

Parking

There shall be no requirement for provision of parking for existing buildings within the Town Center Core overlay zone. No curb cuts shall be permitted on South Union Street. Maximum curb width for a single driveway shall be 12'. Maximum curb cut height for a one-way driveway access shall be 12'. Only one 2-way access driveway, or 2 one-way access driveways shall be permitted for each lot. Where parking lots about a public sidewalk, the parking lot shall be visually screened from the sidewalk by fencing and landscaping sufficient to screen the parked cars. Chain link fencing shall not be permitted.

Parking Provision

Base zoning requirements shall be modified as followings:

Minimum Parking Provisions

Apartments: 0.5 spaces per unit.

Loading and Service

No off-street loading space shall be required for a lot that has frontage on only one street, and does not have rear lane service access.

Outdoor dumpsters and trash bins shall be screened from view on all four sides with fences, masonry work and/or landscaping. chain link fence shall not be permitted.

Utilities

For new construction, all wire utility services shall be underground from the nearest utility pole. No aerial drops shall be permitted.



Honey Locust tree (left) and Princeton Sentry Ginko tree (right)



Screening of outdoor dumpsters/trash bins

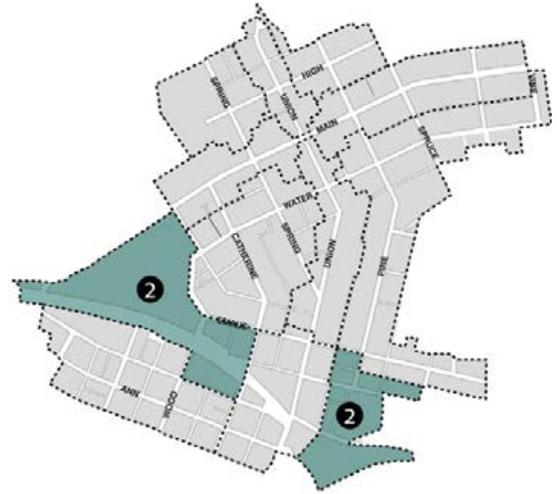
2

EXPANDED TOWN CENTER CORE

The expanded town center core includes development sites west of South Union Street, including the proposed station relocation site, proposed Emaus Street extension and underdeveloped parcels; the existing railroad station; and underdeveloped commercial properties along east Brown Street and East Emaus Street, east of Poplar Street.

Existing Character

The underdeveloped parcels along the rail line to the west of Catherine Street, include the site of the proposed station relocation. Historically, these areas had been developed for railroad related industrial uses. With the growth of the Penn State Harrisburg campus west of Main Street, less than a 15-minute walk to the west, and proximity to the airport, underdeveloped parcels west of Catherine Street offer the opportunity for higher density mixed-use development. Future relocation of the station may provide the opportunity for redevelopment of the existing station site. The 8.5-acre former Middletown Car Works site bounded by Main, Wood, and the railroad is the most significant development opportunity site in Middletown. It is important that development standards and guidelines are in place to assure that new development provides the maximum opportunity to enhance economic activity in the historic town center core. The parcel is currently zoned C1, Commercial District Shopping. The site is located across Main Street from the 3,500-student Penn State Harrisburg campus, and includes the proposed AMTRAK station site. At present, the site is occupied by the auto-oriented Middletown Shopping Center fronting Main Street and a recently cleared industrial parcel at the west end of the site. The shopping center accommodates a drive-through Hardees at the corner of Main and Nissley, and a marginal shopping center that includes a Wells Fargo Bank, Family Dollar Store, and Post Office. In 2011, a developer proposed redevelopment of the site as the Westpointe Center, a larger auto-oriented strip retail center that included a 60,000 sq.ft. full-size grocery store, general retail, pharmacy and pad sites with parking off Main Street. Service access was from Wood and Emaus streets. The site design incorporated an extension of Emaus Street to the west to connect to Main Street. Parking for the proposed new station was provided on the south side of extended Emaus Street. The proposed development included a small 13,000 sq.ft. office building, but no residential development. The development as proposed would have done little to enhance the town center core. The proposed development was auto-oriented with no pedestrian access from public sidewalks. The Emaus Street extension was proposed as a parking access and service drive, with minimal sidewalks, no amenities, and access driveways that crossed the sidewalks.



Proposed AMTRAK station relocation site



New connection via Emaus Street extension between Penn State Harrisburg and the downtown core

There was no attempt to create a more pedestrian-oriented development that also built upon proximity to the new station. If built, it would have been a lost opportunity for the Borough. The shopping center as proposed exceed allowable lot coverage (91% as opposed to the 80% allowed) and did not meet current zoning requirements for parking lot landscaping.

More recently, conceptual designs were prepared for the Borough that incorporated a smaller auto-oriented shopping center, but also included some retail and 3-story residential mixed-use buildings built close to the new Emaus Street sidewalk to create a more inviting pedestrian experience along the Emaus Street extension.

It is important that appropriate design standards and guidelines be prepared for this parcel to encourage an appropriate mix of uses, and assure appropriate building orientation, scale, massing and placement within this development, so that it can function as an integral part of the town center and provide an attractive link between the historic core of Middletown and the Penn State campus and proposed new AMTRAK station.

Commercial development on Brown Street and East Emaus Street east of South Union Street includes modern unattractive single-story buildings, including the marginal shopping Center on Brown at Pine, and the developments on the south side of East Emaus Street, between Astor and Pine. These underdeveloped sites offer the opportunity for more intense mixed-use developments.



Conceptual design prepared by the Borough for the Amtrak Station Relocation site



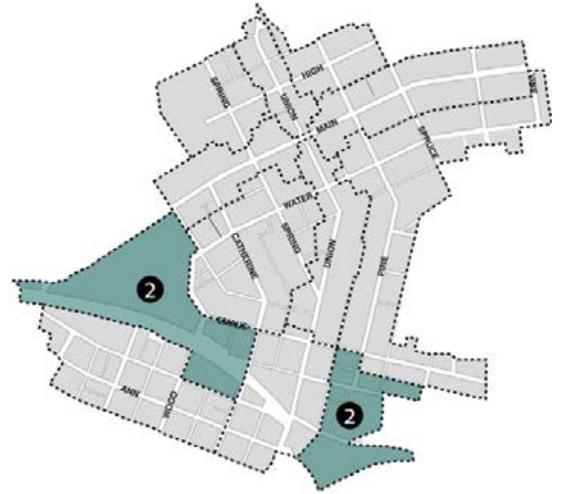
Example of a 4 story mixed use development



Unattractive commercial development on East Emaus Street

Development Goals

- Create pedestrian-friendly streets and blocks with high-quality streetscapes that provide strong visual and pedestrian linkages between abutting residential neighborhoods and the town center core.
- Encourage higher intensity mixed-used development on vacant and underdeveloped sites.
- Create high-quality, well-designed, attractive buildings that provide visual interest at the pedestrian-scale that responds to the traditional Middletown Town Center context.
- Encourage a mix of uses, including commercial and residential uses, to create a 24-hour community.
- Minimize visual impacts of parking, loading and service facilities on streets and pedestrian areas.



Overlay Regulations and Design Standards and Guidelines

Uses & Dimensional Standards

The goal of the Town Center Core Expansion Zoning Overlay is to encourage higher density, multiple use, pedestrian-oriented development and more economically productive use of land parcels, not dominated by automobile uses.

TABLE 2: Zone 2 Permitted Uses and Dimensional Standards

Prohibited Uses	Automobile sales Automobile service stations Automobile service garages Mini-warehouses Drive through restaurants or other drive-through uses	
Permitted Uses	Residential uses as a primary use, where such use is in a building that is part of a multi-building, multiple-use development that includes other permitted commercial uses.	
Dimensional Standards	Lot Area	Minimum lot area for any new parcel subdivision shall be 10,000 sq.ft. for all permitted uses
	Front Yard	No front yard is required except for buildings fronting Main Street, west of Nissley Street, where a 10' front yard setback is required. Maximum front yard setback is 10' except on Main Street. The front yard setback may be extended an additional 20 feet for a portion of a building, if the additional area is used to create an outdoor dining area, or where a public gathering place is provided between the lot line and building entrance. Such setback shall not exceed 40 feet in width.
	Building Height	Maximum building height for a single use building shall be four stories, or 50 feet. Maximum height for any mixed-use building, including a residential building with first floor commercial shall be 5 stories or 60 feet. For a mixed-use building, the minor use must occupy a minimum of 10% of the total area of the building. For a 5-story building, the top floor shall be setback a minimum of 5' from the main plan of the building. The minimum building height shall be 25' for all permitted uses. Roofs shall be flat, consistent with the character of the Middletown Town Center Core.
	Lot Frontage	Buildings shall occupy a minimum 50% of the width of the lot.

DIAGRAM 3: Front Yard and Building Height
Single Use Building

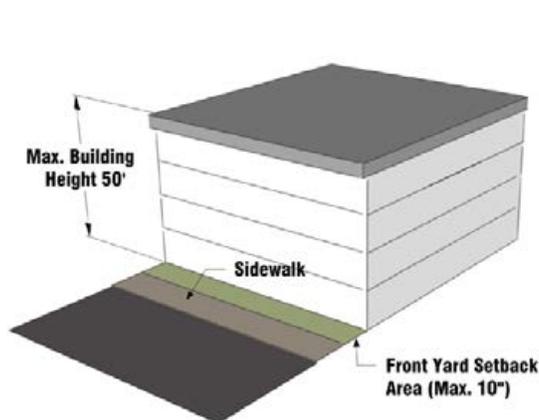


DIAGRAM 4: Front Yard and Building Height
Multi-Use Building

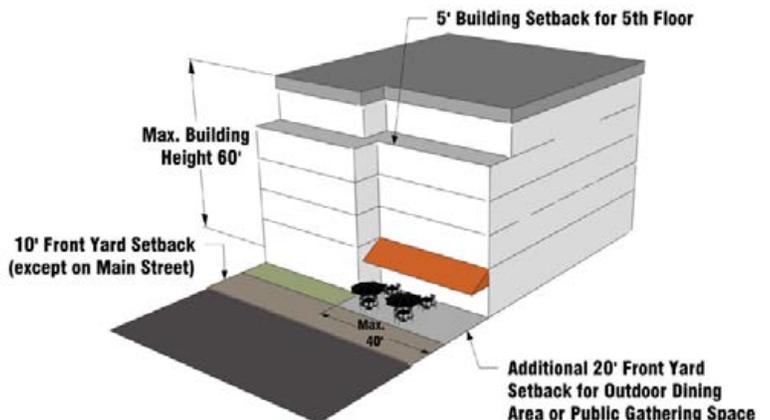
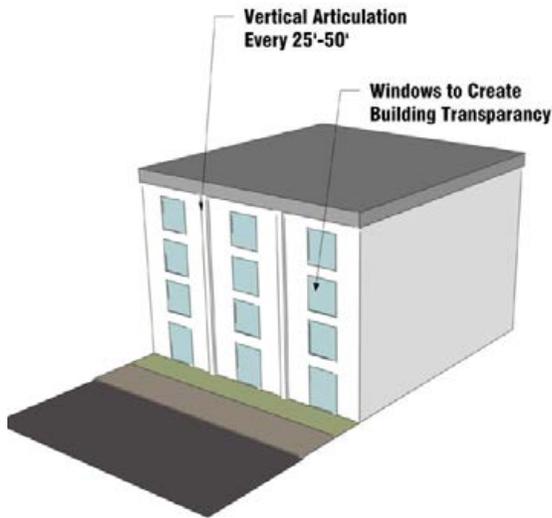


DIAGRAM 5: Vertical Articulation**Building Design Standards**

Within the overall building massing permitted under zoning, articulation shall be provided to break up the mass of the building and create an attractive pedestrian-friendly environment on street facing facades. Street facades shall be designed for visual interest at the pedestrian scale. Long undifferentiated street walls shall be avoided.

Vertical articulation of the building facade shall be provided through changes in plane and/or materials, or projecting features such as bays, balconies, or other similar features. Vertical articulations should be provided that corresponds to the spacing of traditional buildings in Middletown, 25'-50'. Any changes in plane shall be a minimum of 18". Long blank walls facing public streets shall be prohibited. Where feasible, windows shall be provided to offer transparency into building interiors.

Horizontal articulation of the building facade shall be provided to emphasize the base, middle and top of the building, consistent with the character of traditional buildings within the Town Center Core. Such articulation may be achieved through changes in building plane, setbacks, roof terraces, changes in fenestration, or other similar design features to create visual interest and the pedestrian scale. The sidewalk level of the building shall be differentiated through changes in materials, plane of facade, amount of glazing, or other similar means. Stoops or multiple building entrances at first floor are encouraged for residential buildings. Any portion of a building above 38' in height shall be setback in place a minimum of 2 feet and differentiated from the building facade below with changes in building materials and/or fenestration patterns.

Building entrances shall be clearly identifiable and visible from the street and shall be emphasized through a combination of design features such as prominent doorways, and surrounds, entrance canopies, glazed lobbies, changes in facade plane or materials, etc.

For buildings with first floor retail or commercial uses, a minimum 50% of the ground floor facing any public street shall be transparent windows.

The second story and above of facades facing a public street shall contain a minimum of 20% of the facade as clear windows.

Where blank walls are unavoidable for programmatic reasons, such walls shall receive special design treatment to create pedestrian interest. Such treatment may include articulation of the wall surface, changes in materials or wall planes, and artwork or other devices.

For buildings with first floor retail and commercial uses, entrances at a minimum spacing of 50' are encouraged to promote pedestrian activity along the route.

For building with a residential first floor, raised stoops and front doors that provide direct access to first floor units are encouraged to provide visual interest and identity.

Exterior wall materials shall be high-quality materials that are consistent with the traditional building fabric of the Town Center Core, and may include brick masonry, stone, cement stucco, metal, glass, and precast concrete. Fiber cement siding and fiber cement panels are permitted above first floor level. Painted and unpainted plain concrete masonry units, vinyl siding and other residential quality materials are prohibited.

Site Design

Streets

Large land parcels exceeding 5-acres in size shall be subdivided into pedestrian scale blocks with dedicated public access streets. Development of the former Middletown Car Works (Westpointe) parcel shall require the construction of an extension of West Emaus Street from Wood Street to Main Street and dedication as a public street. New streets shall support multiple transportation modes, including vehicles, bicycles, and pedestrians, and accommodate off-street parking. Streets shall be designed to minimize pedestrian vehicular conflicts.

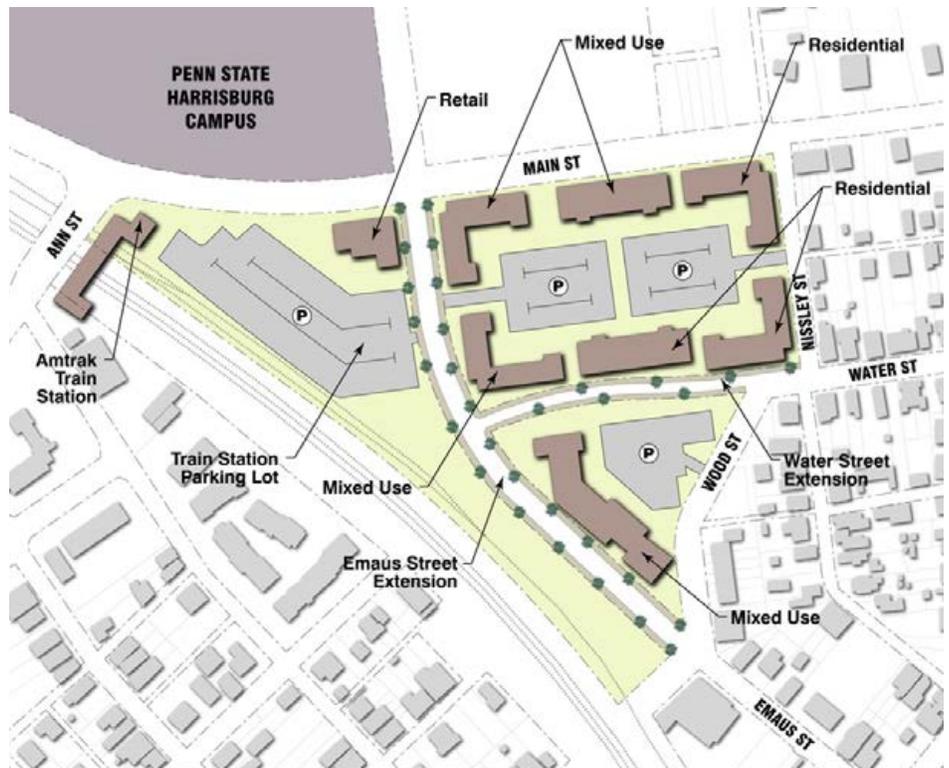
New public and private streets, including the Emaus Street extension, shall have a right-of-way of 60 feet with a 36' cartway and 2 -12' sidewalks. The 36' cartway shall accommodate 2 -8' parking lanes and 2 - 10' moving lanes. The 2, 12-foot sidewalks shall incorporate a 4' wide street furnishings/street tree planting/street lighting zone, adjacent the curb and a clear 8" public pedestrian walking zone.

Sidewalk paving for all public and private streets shall be continuous across any driveway curb cuts. The driveway slope shall be accommodated with the 4' width of the street furnishings zone, such that an 8' wide continuous walkway with a minimum cross slope of 2% extends across the drive.

Rear lane access rights-of-way, if provided, shall be 24' wide for a 2-way vehicular right-of-way.

Maximum curb cut for a 2-lane driveway access shall be 24'. Maximum curb cut length for a 1-way driveway access shall be 12'.

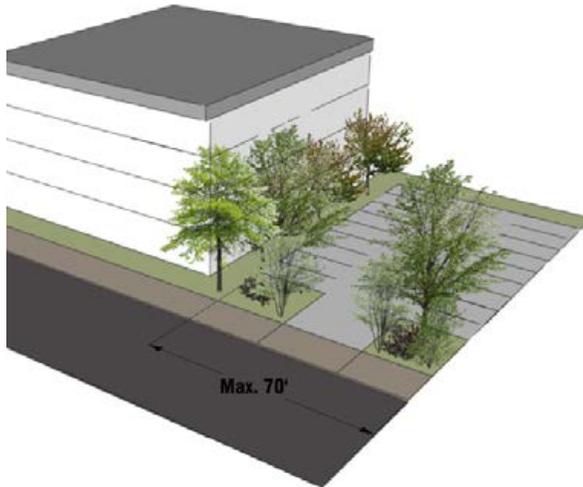
Any new construction shall require the planting of street trees, minimum 2" caliper, at a minimum 30' on center.



Conceptual site plan prepared by KSK for alternative mixed use development with the relocated Amtrak station

DIAGRAM 6: Surface Parking Buffer

Located Next to Buildings



Parking Lots

Parking lots, driveways, loading/unloading areas, and other auto-related functions are prohibited in front of buildings facing the Emaus Street extension. Access driveways shall not pass in front of buildings except for primary pick-up and drop-off for hotel guests.

Clear raised pedestrian walkways with a minimum width of 5 feet shall be provided within parking lots connecting parking to building entrances. Walkways shall run parallel to rows of parked cars. Where walkways intersect auto travel lanes, the crossing shall be clearly delineated through striping or changes in materials.

Where surface parking lots are located to the side of a building the parking lot shall be visually screened from the sidewalk of the abutting street by fencing and landscaping, sufficient to screen the parked cars. Chain link fence shall not be permitted. Off-street parking shall not extend more than 70 feet along any public or private street.

On a corner lot, if surface parking and loading cannot be provided behind the buildings, then parking and loading shall be located along the least traveled street and screened from view.

Loading

Building loading and service functions shall be located within the building where feasible.

All loading and service area shall be located and designed to minimize visual impacts and their presence along public sidewalks. All loading service areas shall be screened for view from public sidewalks by service doors, or fencing and landscaping. No loading/service facilities or trash or recycling containers shall be visible from the Emaus Street extension.

Loading/service access driveways shall be shared with parking access driveways, where feasible.

Parking

Base Zoning Parking Requirements in Section 116 shall be modified as shown in Table 3.

No off-street loading space shall be required for a lot which has only frontage on one street, and has no rear lane service access.

Utilities

For new construction, all wire utility services shall be underground from the nearest utility pole. All primary electrical services shall be located underground for new streets constructed within the town center expansion area.

TABLE 3: Zone 2 Parking Standards

Office and Retail	1 space per 400 sq.ft.
Restaurants/ Taverns and Night Clubs	1 space per 250 sq.ft.
Apartments	0.5 spaces per unit
Hotels	1 space per room

3

MAIN STREET CORRIDOR

Existing Character

Main Street is one of the original streets of Middletown, laid out by George Fisher in 1755. A number of late-18th century and early-19th century buildings remain. Main Street is a “gateway” to Middletown from Harrisburg and the airport, via Route 230 and the airport connection. It is also a gateway from historic Elizabethtown from the east, and from Lancaster from Route 283. The historic crossroads of Main Street, and the gateway to the town center, is at its junction with Union Street, where the intersection was laid out as a classic Pennsylvania Center Square. Originally, the square was defined by the building facades on all the lot lines forming the square. Historic buildings still define the northwest, northeast, and southwest quadrants of the square. Unfortunately, at the southeast corner, the historic 18th-century Washington Inn was demolished in the 1980’s to make way for an auto-oriented 7-11 convenience store and gas station, compromising the integrity of the square. The development was an as-of-right use under the previous C2 zoning regulations. The Center Square was originally defined by a landscaped traffic circle similar to that in Marietta that remained until the 1950s, and gave the square an identity that it lacks today. The identity of the “square” has also been compromised by the suburban character landscaped brick planters at each corner that follow the current curb line. The historic building fabric of Main Street extends from Nissley Street in the west to Vine Street in the east. West of Nissley Street is the auto-oriented Middletown Shopping Center on the site of the old Middletown Car Works. East of Vine Street is the auto-oriented Middletown Plaza Shopping Center.

Main Street parcels west of Union Street are zoned R2, residential multi-family, a zone that permits a wide range of residential uses, including single-family and two-family dwellings, townhouses, and student housing. There are non-conforming auto-oriented commercial uses within the R2 zone, west of Union Street, including single-story auto service facilities set behind parking on the southeast and southwest corners of the intersection of Main and Catherine Street that adversely impact the visual character of this location. Parcels on West Main Street west of Nissley are zoned C1, Commercial, Shopping.

Parcels east of Union Street are zoned C2, General Commercial. The C2 zoning does not permit residential uses as-of-right. The previous C2 zoning regulations have encouraged demolition of the original historic building fabric, and the construction of single-story auto-oriented commercial facilities setback from the sidewalk behind parking, particularly at highly visible intersections with north-south cross streets. The intersection at Main and Pine is still defined on all four corners by handsome, high-quality, historic, brick buildings with original standing seam metal roofs. However, the character of other intersections has been adversely



Historic town square



Washington Inn on the town square, now demolished



Historic traffic circle

3 MAIN STREET CORRIDOR



Historic buildings at the intersection of Main and Pine Streets



Gulf Gas Station at Main and Spruce Streets



Large brick building on Main Street west of Union Street



Smaller lots on Main Street east of Pine Street

affected by new commercial development. For instance, the intersection of Main and Spruce streets is now entirely developed with auto-oriented uses on all four corners, with an auto center on the northwest corner, a beverage store on the northeast corner, a Turkey Hill Gas Station and convenience store on the southwest corner, and a Gulf Gas Station on the southeast corner. Similarly, the intersection of Main and Race includes a JJ Pizza fast food restaurant on the southwest corner, and a used car lot on the southeast corner. None of these developments include street trees or landscaping, and each includes multiple, excessively wide curb cuts. The historic Demp's Corner Pub building has been recently demolished, and the northwest and northeast corners are now vacant lots. The northwest corner of Vine Street intersection has been developed with a drive-in bank, while the southeast corner is now a Rite Aid. Parcels east of Vine street outside the overlay boundary are zoned C1, Commercial, Shopping.

The historic Main Street development pattern includes a mix of residential-scale single, twin and attached homes built close to the street line; the exceptions are portions of the north side at Main Street, between Spring and Apple Streets, where large homes are constructed set back from the street. Attached rowhouse groups do not exceed 4 units, or 60 feet in length. Buildings are typically built to the street line, or with small 10' setbacks that accommodate front porches. There is a wide variety of building forms, and a rich variety of architectural treatments that create a consistent, but varied attractive streetscape, where the historic building fabric remains. Buildings are typically 2-1/2-story, pitched roof, frame construction buildings, many with dormer windows. However, there are also larger brick and stone masonry structures closer to Center Square, between Spring Street and Pine Street. Lots between Spring and Pine Street are typically 40'-60' wide, and accommodate buildings 30'-40' wide. Lots are typically smaller east of Pine Street, particularly mid-block lots, 20'-40' wide buildings east of Spruce Street are often not well maintained. The character of Main Street, particularly east of Union Street, is compromised by narrow sidewalks, traffic, and lack of street trees. Areas of historic herringbone pattern to red brick sidewalk remain in some areas.

Development and Design Issues on Main Street

Basic issues within the Main Street Corridor that need to be addressed in the zoning overlay design standards and guidelines include the following:

1. Demolition of Historic Fabric

Demolition of historic fabric, particularly at high-visibility intersections with north-south cross streets, to accommodate auto-oriented commercial uses. Demolition of the fabric to accommodate single-story convenience commercial with buildings setback behind parking lots detracts from the unique

historic character of Main Street. This has been a particular problem on East Main Street, where the C2 zoning has encouraged demolition and auto-oriented development. Under the new code, the configuration of much of this development, with buildings setback behind parking, would not be permitted under the C2 regulations, as no parking is permitted between the building and street line. Automobile service stations and service garages are also not permitted in the C2 zone. However, automobile sale lots are permitted.

2. Non-Conforming Auto-Oriented Commercial Uses

Non-conforming auto-oriented commercial uses setback behind parking at Catherine Street intersection on West Main Street that detracts from the attractive residential character of West Main Street.

3. Lack of A Strong Identity For Center Square

Middletown’s Center Square compares unfavorably with that of the nearby towns, such as Elizabethtown, where the Center Square has retained buildings that define the Square, and the Square is a landmark within the community or with Marietta, where the square is defined by an attractive landscaped traffic circle. The construction of the single-story 7-11 setback from the lot line has significantly impacted the integrity of Center Square. The low brick planters following the curb line compromise the concept of a “town square”.

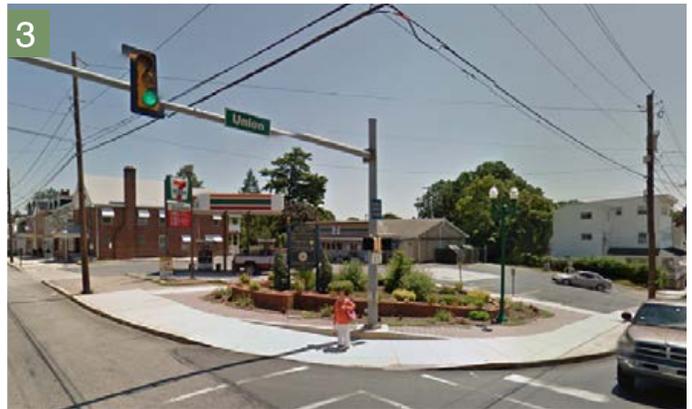
The paved areas appear at each quadrant appear as leftover space. These open underutilized areas could provide the opportunity for outdoor café seating or other uses, as in Elizabethtown.

4. Lack of Screening, Landscaping, and Street Trees

Lack of adequate screening of parking areas detract from historic character of Main Street. There is also a lack of landscaping and street trees, to provide shade, particularly on East Main Street. Historic photos show former extensive street tree planting on Main Street, particularly West Main Street. They were removed by PennDOT in the 1950s as part of a road widening project that also included elimination of the traffic circle.

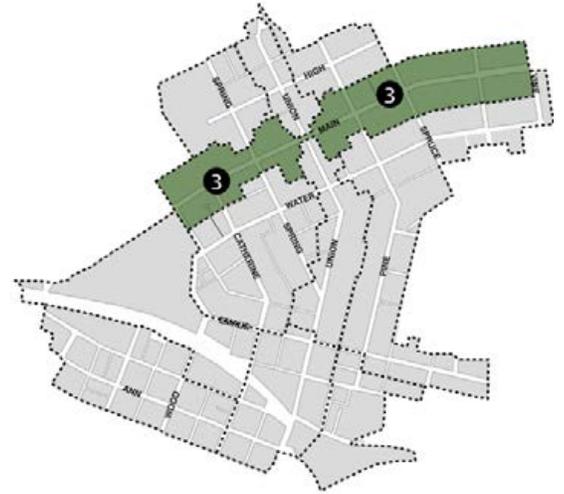
5. Excessively Wide Cartway And Narrow Sidewalks

Excessively wide cartway and narrow sidewalks discourage pedestrian activity along Main Street. The Main Street right-of-way is 65 feet wide, with a 45’ cartway and 2’-10’ sidewalks. A 40’ wide cartway is adequate to accommodate 2 moving and 2 parking lanes, providing the opportunity to widen the sidewalks, to 12’, to more easily accommodate street trees, and create a more comfortable environment for pedestrians. Sidewalk bump-outs within the parking lanes and at intersections, and mid-block would provide an opportunity for landscaping and tree planting, and create a more attractive pedestrian environment.



Development Goals

- Protect and reinforce the historic character of Main Street, including Center Square. Encourage retention and rehabilitation of historic buildings.
- Protect and enhance the residential character of West Main Street and the mixed-use residential/commercial character of East Main Street.
- Encourage the appropriate redevelopment of inappropriate non-conforming developments that compromise the character of the district. Encourage the development of Center Square as a destination, with attractive outdoor public spaces, and active uses.
- Encourage streetscape improvements to Main Street that will



provide a more pedestrian-friendly environment, including widened sidewalks and tree planting.

- Encourage appropriate development of accessory residential units on rear lanes to Main Street properties.

Overlay Regulations and Design Standards and Guidelines

Uses, Dimensional Standards, and Parking

The goal of the Main Street zoning overlay is to encourage retention and rehabilitation of the existing historic building fabric, protect residential uses, and encourage appropriate redevelopment of vacant and inappropriately development sites, as mixed-use developments consistent with the historic building fabric.

Building Design Standards and Guidelines

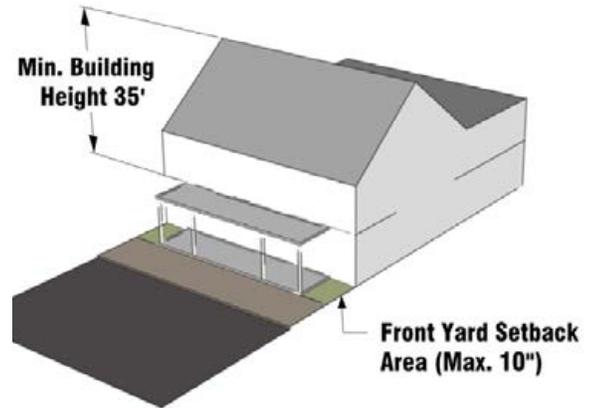
Demolition

No building identified as key of historical or architectural interest may be demolished without Planning Committee review, public hearing, and Zoning Hearing Board approval.

TABLE 4: Zone 3 Permitted Uses and Dimensional Standards

<p>Prohibited Uses (C2 Zoned Parcels)</p>	<p>Automobile Sales Parking structures, unless located to the rear of the lot, behind an active use, and not visible from Main Street Restaurants that include a drive-through facility Retail store that includes a drive-through facility</p>	
<p>Permitted Uses (C2 Zoned Parcels)</p>	<p>All residential uses permitted in any residential district, with the exception of residential conversions.</p>	
<p>Dimensional Standards</p>	<p><i>Front Yard</i></p>	<p>Maximum front-yard setback under the Main Street Zoning Overlay is 10', except where adjacent existing buildings are built to the street line, in which case the maximum front yard setback is zero feet.</p>
	<p><i>Minimum Building Height</i></p>	<p>Minimum building height for all uses is 2 stories and 25 feet.</p>

DIAGRAM 7: Front Yard and Building Height

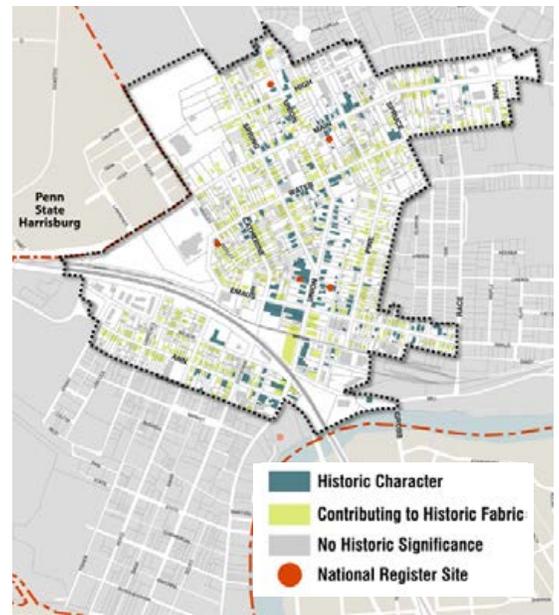


Rehabilitation

Repairs and rehabilitation, alterations, additions, or adaptive reuse of buildings within the overlay identified as of historic and/or architectural interest or contributing to the character of Main Street corridor, shall respect the original design character, and materials of the building. Inappropriate changes to street facing facades such as reducing the size of window openings, remaining original decorative or covering original materials, shall be prohibited.

New Construction

Exterior wall materials shall be high-quality materials and may include brick masonry, stone, cement stucco, and glass. Painted or unpainted concrete masonry units, or vinyl siding are prohibited. Within the overall masonry envelope permitted under the overlay zoning, and building placed on the lot, new construction shall respect the character of the historic building fabric. Articulation of the building facade shall be provided to create an attractive pedestrian environment on street facing facades.



Map of Historic Character and Contributing Historic Fabric



Example of appropriate adaptive reuse of a historic building in Philadelphia, PA

TABLE 5: Zone 3 Parking Standards

Detached and semi-detached single-family and 2-family and attached dwellings	1 space per dwelling unit
Apartment buildings, including students	.75 spaces per dwelling unit
Tourist Homes, Hotels	1 space per room

Streetscape

Street Trees

Any new construction or addition shall require the planting of street trees, minimum 2” caliper, at a minimum of 30 feet on center.

Sidewalks

For new construction, sidewalks shall be a minimum 10’ wide, including a minimum 6’ walk zone and 4’ tree planting zone. New buildings shall be setback sufficiently a portion of which may be located behind the lot line.

Parking

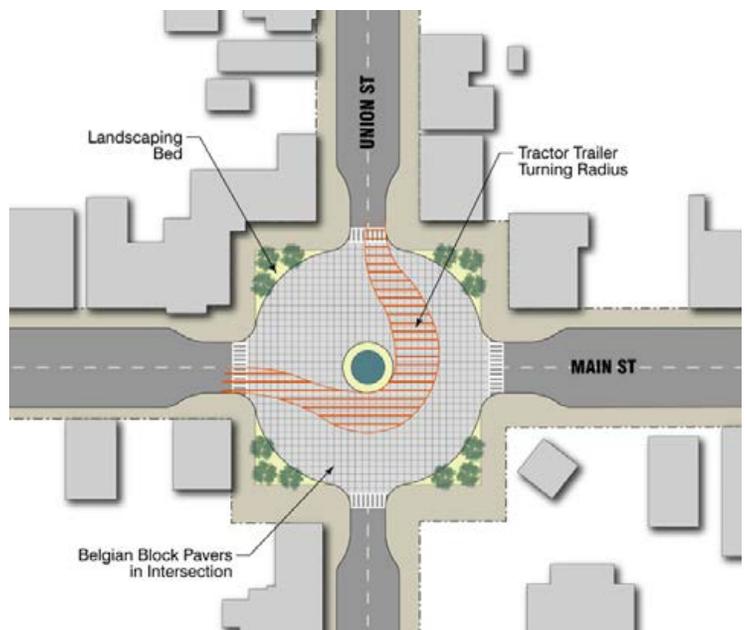
Within the Main Street Overlay District, base off-street zoning parking requirements under Article 16 are modified in Table 4.

Off-street parking shall be screened from views from Main Street with fencing of an appropriate design, consistent with the historic building fabric and landscaping within the required 3’ planting strip. Fencing of an appropriate design may include traditional white picket fencing. Chain link fence shall not be permitted.

Curb cuts on Main Street shall be prohibited for corner parcels, or parcels with access to a rear driveway.

Utilities

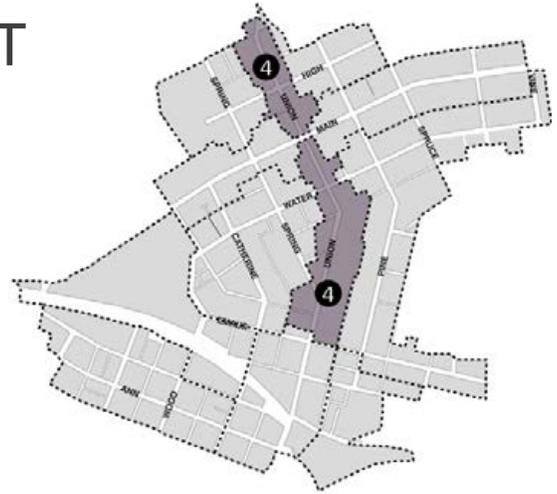
For new construction, all wire utility services shall be underground, from the nearest utility pole.



Example of conceptual redesign of traffic circle at Main and Union Streets

4

NORTH UNION STREET CORRIDOR



Existing Character

North Union Street is one of the original streets of Middletown, laid out by George Fisher in 1755. Union Street is the gateway to Middletown from the interchange of Route 141 with Route 283, and the gateway to the town center commercial core at South Union and Emaus Street. The historic crossroads of Middletown at North Union and Main Street at Center Square is described under the Main Street Corridor section. The historic fabric of North Union Street extends from Carmony Avenue/Park Circle to Emaus Street. The east side of North Union Street, between Park Circle and Water Street and both sides of the section of North Union Street between Emaus and Nissley Street are zoned C2 General Commercial. The remainder of the corridor is zoned R2, Residential Multi-family. The historic North Union Street development pattern between Park Circle and Main Street includes a mix of 2 1/2- to 3-story twin and single residential buildings, with deep front porches and a mix of gable front and eaves front pitched roofs, built with a small setback from the street line. The C2 commercial zoning on the east side of Union Street has encouraged demolition of original buildings, and creation of vacant lots, and construction of newer buildings setback from the street line behind surface parking.

The intersection of North Union and High Street is defined on all four corners by attractive historic buildings, including the handsome stone 1767 St. Peter's Church on the northwest corner, and fine historic homes on each of the other three corners. Sidewalks in both the northwest and southeast corners of intersection have historic red brick sidewalk paving.

The section of North Union Street between Main and Water Street includes a mix of higher density 2- to 3-story pitch and flat roof twins and attached homes, built close to the street line, on 20-40' wide lots. Many mid-block buildings are not well-maintained and have been refaced with vinyl siding. North Union Street at the historic Center Square intersection is adversely impacted by the 7/11 and associated parking lot on North Union Street. The intersection of Union and Water Street is defined by attractive historic buildings, including the Middletown Presbyterian Church at the southeast corner, and an attractive storefront mixed-use, mansard roof building at the northwest corner, and wood frame clapboard house at the southwest corner. The historic mixed-use building on the northeast corner has been compromised by use of low-quality vinyl siding and fake vinyl shutters. While historic photos show this block of North Union with continuous street trees, street widening over the years has reduced the width of sidewalks, and the available space for street trees. There is now little landscaping to soften the environment.

The section of North Union Street between Water and Spring Street includes larger late-19th century single houses, setback



C2 zoned parcel set back from North Union Street on the east side



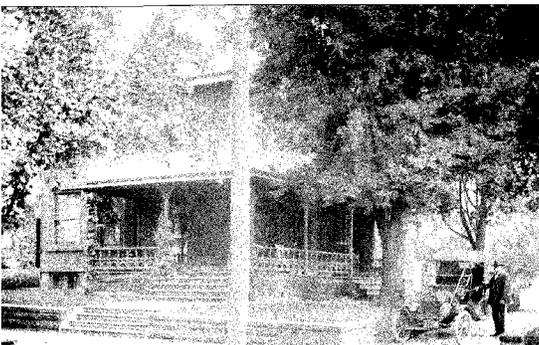
St. Peter's Church



Vinyl siding houses on North Union Street between Water and High Streets



Mixed use building at High and Union Streets



Historic photo of North Union Street



Alfred's Victorian Restaurant



Brownstone Cafe

on the east 30' from the sidewalk, creating landscaped front yard areas. The substantial homes includes bay windows, large porches, dormers and turrets. Lots are typically 40'-60' wide and 200' deep, and are served by rear alleys. Many of the homes on the west side are more modest attractive 2- and 3-story brick front and clapboard homes, built to the back of the sidewalk. The intersection of North Union and Spring Street is defined by the late-19th century St. Peter's Evangelical Lutheran Church, and the angular form of the 3-story brick flat iron building at the northwest corner of Spring and Union Street.

The block of North Union between Spring and Emaus is the most elegant block of Middletown and is lined with grand late-Victorian houses, setback from the sidewalk behind landscaped front yards, and tree-lined sidewalk. Many are now converted to commercial use, including Alfred's Victorian Restaurant. The intersection of North Union and Emaus is the heart of the commercial core and is defined by the Brownstone Café in the old Farmers Bank Building and dock in the northwest corner, the Elks Building in the southwest corner, and the Rewalt Building at the southeast corner.

North Union Street Development Design Issues

Basic issues within the North Union Street corridor that need to be addressed in the zoning overlay regulations design standards and guidelines include the following:

1. Protection of Historic Building Fabric

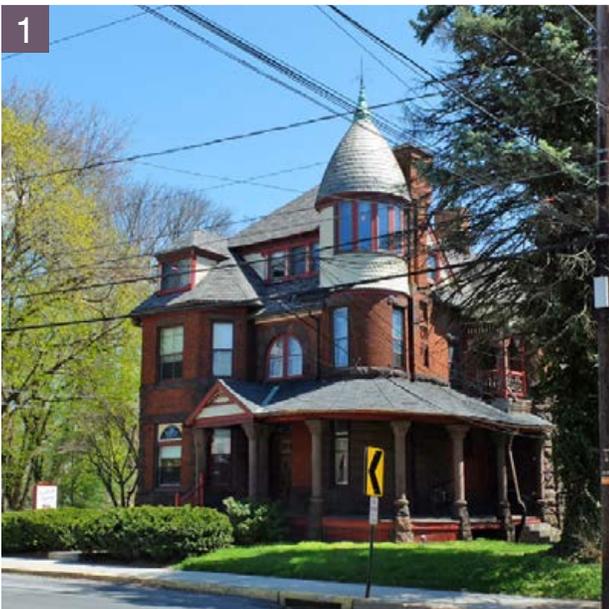
North Union Street is the "gateway" to the Borough core and includes many historic buildings, from the late-18th century at High Street through the late-19th and early-20th century buildings near Emaus Street. In some area, such as at the intersection at Center Square with Main Street, the previous C2 zoning encouraged demolition of a historic building and construction of auto-retail development setback from the sidewalk. Historic buildings at key intersections at High Street and Water Street could be threatened in the future. Protection and enhancement of the residential character of North Union Street north of Nissley Street is also needed.

2. Inappropriate Rehabilitations/Repairs to Historically and Architectural Interesting Buildings

Issues including refacing of historic wood frame, wood siding-facaded buildings with vinyl siding, reduction in size of window openings, vinyl windows, fake vinyl shutters, and replacement of original solid porch columns with light gauge metal supports, changes which detract from the integrity of the streetscape.

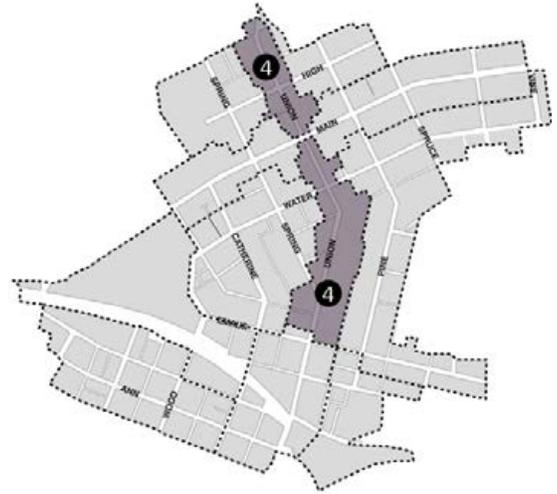
3. Lack of Street Trees and Landscaping

A lack of greenery can be found in sections of the corridor, particularly between Main and Water streets.



Development Goals

- Protect and reinforce the historic character of North Union Street. Encourage retention of and rehabilitation of historic buildings.
- Protect and enhance the residential character of North Union Street. Protect single-family occupancy.
- Encourage appropriate development of accessory residential units on rear lanes.



Overlay Regulations and Design Standards and Guidelines

Uses & Dimensional Standards

The goal of the Union Street zoning overlay is to encourage retention and rehabilitation of the existing historic fabric, and encourage a mix of residential and small-scale commercial uses and appropriate redevelopment of vacant and inappropriately developed sites as residential and mixed-use developments consistent with the historic building fabric.

TABLE 6: Zone 4 Prohibited/Permitted Uses & Dimensional Standards

Prohibited Uses (C2 Parcels)	Parking structures, unless located to the rear of the lot, behind an active use, and not visible from Union Street. Restaurants that includes a drive-through facility. Retail store that includes a drive-through facility.	
Permitted Uses (C2 Parcels)	All residential uses permitted in a residential district, except for residential conversions.	
Dimensional Standards	Front Yard	Front yard setback shall be consistent with adjacent properties, and shall be no more or less than the average of the two abutting parcels.
	Minimum Building Height	Minimum building height for all uses is three stories, and 35 feet
	Lot Frontage	Buildings shall occupy a minimum 50% of the width of the lot.

DIAGRAM 8: Front Yard and Building Height



Building Design Standards and Guidelines

Demolition

No building within the North Union Street Overlay identified as being of historical or architectural interest shall be demolished without Planning Committee review a public hearing, and approval of the Zoning Hearing Board.

Rehabilitation, alterations, additions to, and adaptive reuse of buildings within the Overlay, identified as historical and/or architectural interest, and contributing to the character of the North Union Street Corridor, shall respect the original design character and materials of the building. Inappropriate changes to street facing facades such as reducing the size of window openings, or covering original materials with vinyl siding, shall be avoided.

Streetscape

Street Trees

Street trees shall be planted in the sidewalk at a minimum 30 feet on center.

Sidewalk Paving

Existing brick paved sidewalks shall be repaired or replaced with matching materials.

DIAGRAM 9: Building Features to be Preserved



Example of street trees



Existing paved sidewalks on Union Street



Example of historic style fencing

Parking

Within the North Union Street Overlay District, base parking requirements under Article 16 are modified as shown in Table 7 below.

TABLE 7: Zone 4 Parking Requirements

Single-family and 2 family dwellings	1 space per dwelling unit
Apartment buildings, including student housing	.75 spaces per dwelling unit
Townhouses/hotels	1 space per room

Off-street parking shall be screened from views from North Union Street and intersecting side streets with fencing and landscaping within the required 3' planting strip. Fencing shall be consistent with the historic character of Middletown and shall conform to the design guidelines. Chain link fence shall not be permitted.

Driveway curb cuts on North Union Street shall be prohibited for corner parcels with cross-street access, and parcels with access to a rear driveway.

Utilities

For new construction, all wire utility services shall be underground from the nearest utility pole.

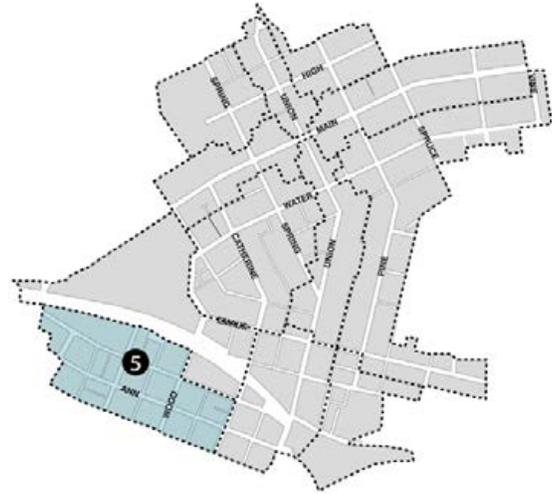
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ANN STREET CORRIDOR

Ann Street is an important southern link over the AMTRAK line that connects Main Street/West Harrisburg Pike near the Penn State Harrisburg campus, with Route 141 and Susquehanna riverside communities to the south. Ann Street forms part of the triangle formed by the three streets; West Main, North Union and Ann Street that define the town center.

The Ann Street Corridor character district within the zoning overlay boundaries extends from South Union Street in the east, to Grant Street in the west. The corridor is bounded by Witherspoon Street in the south and to the north by the AMTRAK line and Wilson Street. The area is characterized by small-scale 200' by 200' blocks. For most of its length, Witherspoon Street functions as a rear alley for the 200' deep Ann Street lots and lots fronting Market Street to the south. At some locations, such as between South Catherine and Swatara streets, accessory dwellings have been developed on the rear of the lot fronting the north side of Witherspoon Street. Ann Street is a potentially attractive, predominantly residential street with a rich variety of buildings, but is presently in a deteriorated condition for much of its length.

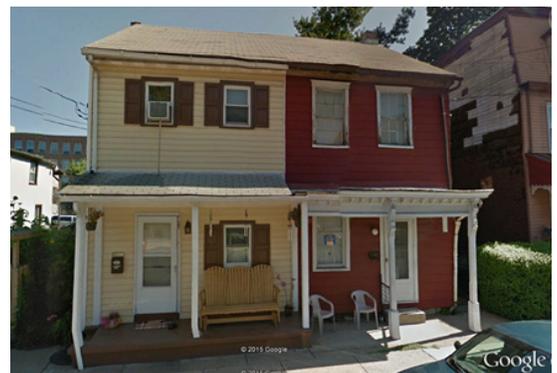
The two blocks of Ann Street between South Union and South Catherine are zoned C2, General Commercial and form part of the town center core. Blocks to the west are zoned R2, Multi-family Residential. The two blocks between South Union and Catherine Street include a number of historically and architecturally interesting 2- and 3-story buildings, built at or close to the street line. Buildings include handsome brick single and attached residential buildings, particularly in the south side. The south side of Ann Street, between South Union and Scott Street, is located within the 100-year floodplain. The high visibility northwest corner of the intersection of Ann and Union is disfigured by a poorly altered, but originally handsome mixed-use building where the facade has been covered in vinyl siding, and original window openings reduced in size. The unsightly parking lot to the rear is not screened from the sidewalk, and there is no landscaping. West of Catherine to Wood there is a mix of single, twin, and attached 2- and 3-story residential buildings, many of architectural and historical interest. While the original fabric is intact or many of the buildings, others have been poorly altered with the addition of vinyl siding, and removal of original solid porch columns and replaced with metal supports. While early photographs of the street show extensive street tree plantings, the street is now mostly devoid of trees. Buildings include a mix of 3-story flat roof wood frame buildings with bracketed cornices, some with mansard 3rd stories, and 2-story, wood frame pitch roof buildings. Most buildings have shallow front porches, separating buildings from the sidewalk. West of Wood Street to Grant Street, less of the historic building fabric of Ann Street remains, and building conditions are more deteriorated. The southwest



Ann Street



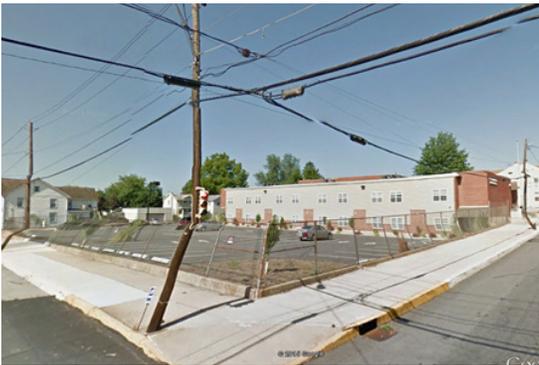
Historic buildings on Ann Street between Union and Catherine Streets



Renovations to reduce window size and cover brick facade with vinyl siding on Ann Street



Lack of street trees or landscaping on Ann Street



Unscreened school parking lot on Ann Street



Poor building condition and reduced size of windows on Ann Street west of Wood



Redevelopment at Ann and Grant Streets with little articulation or porches/protective canopies

corner of Ann and Wood Street is occupied by a large unsightly parking lot of the Mansberger Elementary School, surrounded by a high chain link fence, without any mitigating landscaping or street trees. Buildings west of Wood are often poorly altered with original window openings reduced in size or eliminated. Newer construction at the northeast corner of Ann and Grant and Grant and Wilson does not contribute to the character of the corridor and has an adverse visual impact on the district. The flat fronted houses provide no definition of individual entrances that could have been provided by porches or protective canopies. At the corner of Grant and Ann, newer houses are setback from Grant Street behind unsightly chain link fencing. At Grant and Wilson barrack-like housing blocks without articulation have excessive setbacks, and front yard areas are devoid of landscaping. There are no street trees.

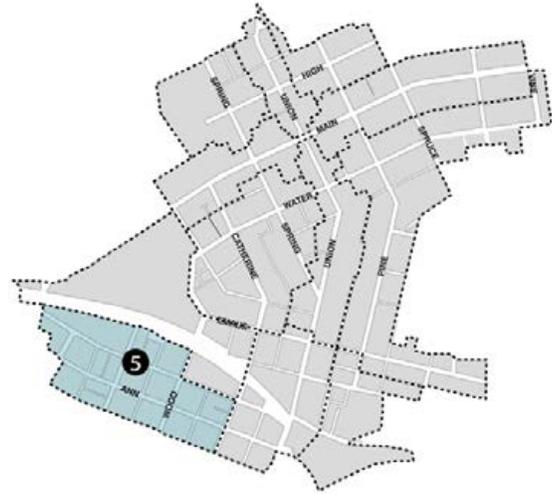
Ann Street Development Design Issues

1. **Deterioration of the historically and architecturally interesting building fabric, particularly west of Wood Street.**
2. **Poor quality repairs, alterations, and improvements that detract from the architectural character of the buildings, including elimination of porches and window trim, reduction in size of windows, and removal of original porch columns.**
3. **Lack of street trees and landscaping to soften the urban environment.**
4. **Unsightly unscreened parking lots without landscaping.**
5. **Deep lots on south side of Anne Street, with rear access from Witherspoon Street may offer the opportunity for development of accessory residential units fronting Witherspoon Street.**
6. **Unattractive barracks-like new residential development at the west end of Ann Street that does not reflect the character of Ann Street.**



Development Goals

- Retention and rehabilitation of historic building fabric.
- Encourage single-family owner occupied housing.
- Require that new construction respect the historic character of Ann Street.
- Encourage accessory dwelling units on rear lanes.



Revitalization Overlay Regulations, Design Standards and Guidelines

Uses

The goal of the Ann Street Zoning Overlay is to encourage the retention and rehabilitation of the deteriorating pedestrian-scale historic building fabric, for predominantly single-family and multi-family residential use on blocks west of Scott Street, and single-family and multi-family residential/commercial mixed uses in the block between Scott and South Union Street.

TABLE 8: Prohibited/Permitted Uses

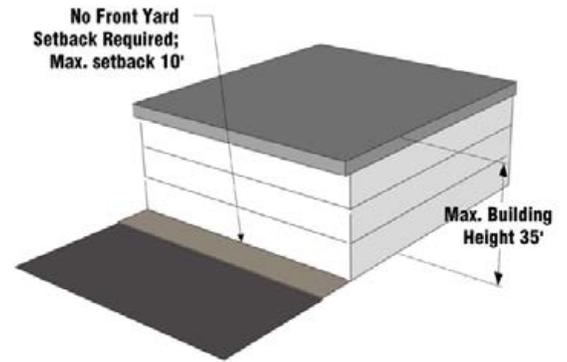
Prohibited Uses	C2 Parcels	Automobile sales Parking structures Drive-through restaurants and other drive-through retail facilities
	R2 Parcels	Manufactured home parks
Permitted Uses (C2 Zoned Parcels)	All residential uses permitted in any residential district, except residential conversions.	

Dimensional Standards

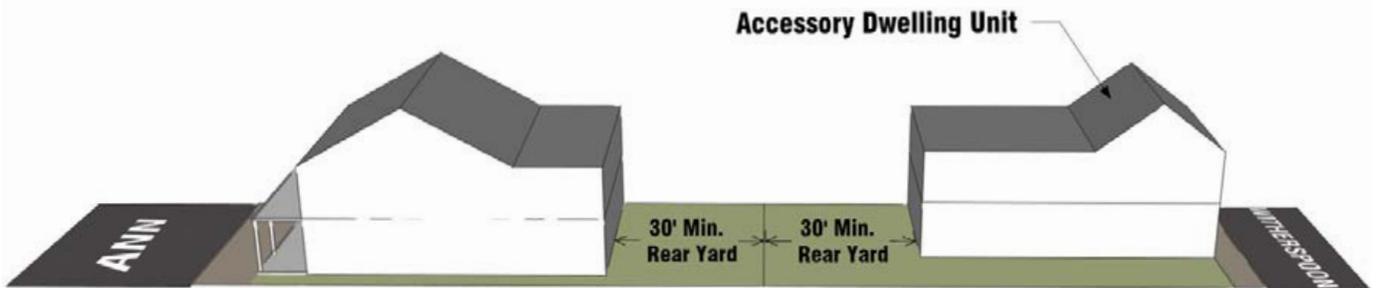
TABLE 9: Zone 5 Dimensional Standards

C2 Parcels	Front Yard	No front yard is required. Maximum front yard setback shall be 10', except where front yards of adjacent properties are greater than 10', in which the maximum setback is equal to the average front yard setback of the abutting properties.
	Maximum Building Height	Maximum building height for all uses is 3 stories or thirty-five (35) feet.
R2 Parcels	Lot Width	Minimum lot width for a single-family dwelling: 40 feet. Minimum lot width for student housing: 60 feet.
	Yards	Where a lot on Ann Street has rear access from Witherspoon Street and an accessory dwelling unit is to be constructed fronting Witherspoon Street, the required rear yard for both the Ann Street and Witherspoon Street may be located between the two buildings. The required rear yard for each building on the lot is 30'.

**DIAGRAM 10: Front Yard and Building Height
C2 Parcel**



**DIAGRAM 11: Accessory Dwelling Unit
R2 Parcel**



Design Standards and Guidelines

Rehabilitation, repairs, alterations, or additions of existing buildings within the overlay district, identified as of historical and/or architectural interest, and contributing to the character of the Ann Street Corridor, shall respect the original design character, details, and materials used in construction of the buildings. For facades fronting public streets, inappropriate changes, such as reducing the size of window openings, eliminating window openings, covering original materials with vinyl siding, removing porches, removing original porch support columns and replacing such columns with metal bar supports, and elimination of decorative trim such as cornice brackets and window-heads and sill is prohibited.

Streetscape

Any new construction, or major rehabilitation of a building on the Ann Street Corridor shall require the planting of street trees, minimum 2" caliper, at a minimum 30' on center within the sidewalk.

Parking

Base Zoning Off-Street Parking Requirements under Article 16 shall be modified as shown in Table 10.

All off-street parking shall be screened for public view from the sidewalk by landscaping and a minimum 42" high fence, of a design consistent with the historic building fabric. Chain link fence is prohibited.

TABLE 10: Zone 5 Parking Standards

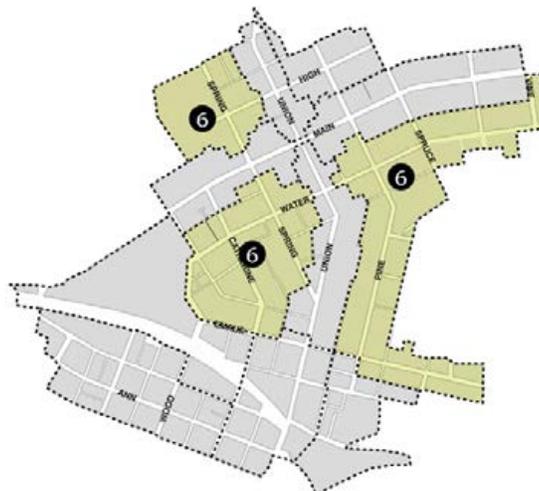
Single-family and 2 family dwellings	1 space per dwelling unit
Apartments/single family attached dwellings	1 space per dwelling unit
Student Housing	1 space per dwelling unit

Utilities

For new construction, all wire utility services shall be underground, from the nearest utility pole.

6

R2 ZONED TOWN CENTER RESIDENTIAL NEIGHBORHOODS



Existing Character

This zone includes the core residential neighborhoods bounded by the main gateway corridors, west and east of North Union Street. There are two principal areas:

1. The traditional residential neighborhood centered on Spring and Water streets, west of Union Street. The area extends from Carmony Street in the north, to the rear parcel lines of Union Street in the east, and Emaus/Wood Streets to the southwest. The neighborhood is bisected by the Main Street Character District.
2. The traditional residential neighborhood east of South Union Street centered on Pine Street, between Water and East Emaus Street and Emaus Street between Astor and Race streets. The area extends from Water Street in the north to the rear lot lines of South Union Street to the west, Peony Street to the east and East Emaus Street to the south. It also includes properties fronting East Emaus Street east to Race Street.



Typical housing on Spring Street north of Main

Principal Area Characteristics

1. Spring Street Neighborhood – West Side

North of Main Street, Spring Street is predominantly single-family residential, with modest, mostly 2-story pitched roof wood frame construction houses. Some are located on larger lots, which provide a more open landscaped setting than blocks south of Main Street. South of Main Street, Spring Street is predominantly single-family residential, with closely spaced 2-story twin and single homes with shallow front porches, built to the street line on relatively narrow lots. The buildings create an attractive walkable residential streetscape. However, some buildings have been compromised by poor quality alterations and repairs, including removal of character defining features, such as decorative cornices, and replacement with original porch columns with metal supports. The high visibility intersection of Spring and Main streets is particularly attractive. Catherine Street south of Water Street and north of Girard and was developed in the late-19th century with several substantial 3-story brick-faced semi-detached houses with wraparound porches. South of Girard, Catherine Street includes a group of closely spaced early 2-story pitched roof frame and siding houses with shallow front porches built to the sidewalk. Nissley Street includes an attractive mix of early single and semi-detached wood frame and siding homes built close to the street line.



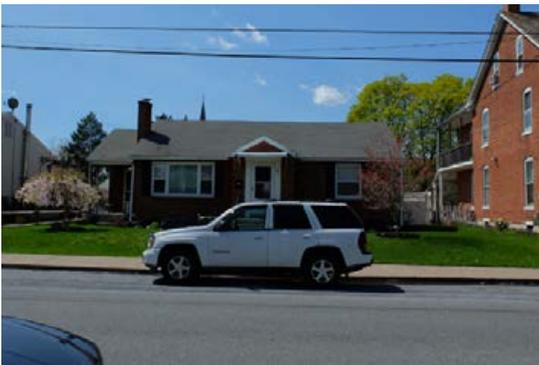
Attractive building at the corner of Spring and Main Streets



3-story brick house on Catherine south of Water Street



Single family home on Pine Street



Building setback too far from the sidewalk



Attractive building at Pine and Water Streets



Typical houses on Emaus west of Peony Street

2. Pine Street Neighborhood – East Side

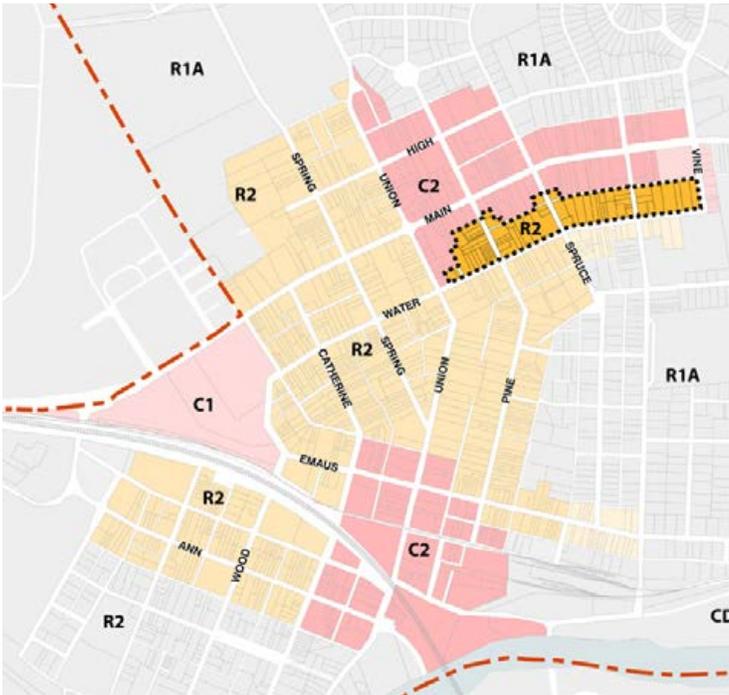
Pine Street between Emaus and Water streets includes predominantly 2- and 3-story late-19th and early-20th century single and semi-detached, brick and frame construction homes on larger lots, with substantial front porches and a small landscaped setback between the front porch and back of sidewalk. In some case, more recent single-story homes have been constructed setback from the sidewalk, disrupting the streetscape. Many homes have gable fronts, or mansard roofs to accommodate the third story. North of Water Street, Pine Street is developed with earlier brick construction and wood frame construction single, semi-detached, and attached homes built to the sidewalk, typically without front porches. The northeast corner of Pine and Water streets is occupied by a handsome Colonial era brick single house, with pitched roof and 3rd floor dormer windows.

Some buildings of architectural and historical interest have been poorly altered, and original character defining features such as porch columns and decorative trim have been removed. Currently this section of Pine Street north to the rear lot lines of parcels fronting Main Street, is located within the C2 zoning district, but should logically be part of the R2 zoning district to the south.

East Emaus Street is developed with predominantly single detached homes, but with some twin homes. Homes east of Peony Street are predominantly early-20th century with large front porches and landscaped setbacks. Homes west of Peony Street are mid- to late-19th century frame houses, with shallow porches, built to the sidewalk. Many have gable fronts.

The properties on the north side of Water Street between Union and Vine Streets are currently zoned C2 Commercial; however, they are almost entirely residential. Buildings include a mix of attractive 2- and 3-story twin and single homes, some with pitched roofs, and others with a 3rd floor mansard and cornices. Buildings are architectural interesting and most have front porches. Most buildings are well-maintained, and the entire street has an attractive residential character. The only incongruous development is the recently constructed unsightly self-storage facility at the northwest corner of Water and Vine Street, where the metal shed building structure is setback behind a parking lot, without landscaping or screening, and adversely impacts the otherwise attractive residential block. C2 zoning for the properties fronting the north side of Water Street is not consistent with their use, and encourages the demolition of residential structures. The north side of Water Street and residential parcels fronting Pine Street, north of Water Street and south of Main Street should be rezoned from C2 Commercial to R2 Multi-family Residential to reflect their current and appropriate residential use. Residential zoning would encourage retention and rehabilitation of the historic building fabric.

DIAGRAM 12: Proposed Rezoning Area



Proposed rezoning of the north side of Water Street from C2 Commercial to R2 Multi-family



Existing residential character on the north side of Water Street (currently zoned C2 Commercial)



Self storage facility at the northwest corner of Pine and Water Streets that was built by-right under existing C2 zoning

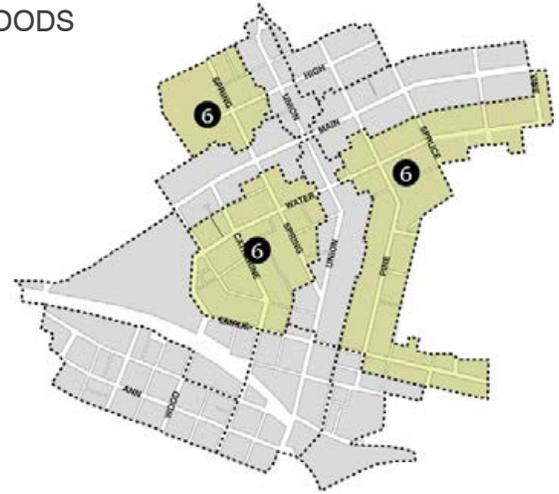
Development Issues

1. Newer construction single-story houses with inappropriately large setbacks that are inconsistent with abutting properties.
2. Poor quality alterations and repairs, particularly north of Columbia Street.
3. Inappropriate C2 zoning for north side of East Water Street.
4. Need to encourage preservation and revitalization of historic building fabric on the north side of Water Street.
5. Deteriorating properties, particularly north of Water Street.
6. Haphazard developed of near alleys, including Peony Avenue.



Development Goals

- Protection and enhancement of single-family owner occupied character of the neighborhood.
- Encourage appropriate repair and rehabilitation of existing housing stock.
- Encourage new development on appropriate sites that respects the existing historical and architectural character of the neighborhood.
- Discourage demolition of buildings of architectural and historical interest.
- Encourage development of accessory dwelling units on rear lanes, including Astor Avenue.



Zoning Overlay Regulations and Design Standards Guidelines

Uses and Dimensional Standards

TABLE 11: Zone 6 Prohibited Uses and Dimensional Standards

Prohibited Uses	Manufactured home parks.	
Dimensional Standards	<i>Minimum Lot Width for a Single Family Dwelling</i>	40'
	<i>Required Building Setback</i>	Required setback is equal to the design front yard setback of abutting buildings, but may not exceed 20 feet.
	<i>Maximum Building Height</i>	Maximum building height for all uses is 3 stories and thirty-five (35) feet.

Building Design Standards and Guidelines

Demolition

No building within the Town Center Residential Neighborhoods identified as being of historical or architectural interest shall be demolished without Planning Committee review, a public hearing, and approval of the Zoning Hearing Board.

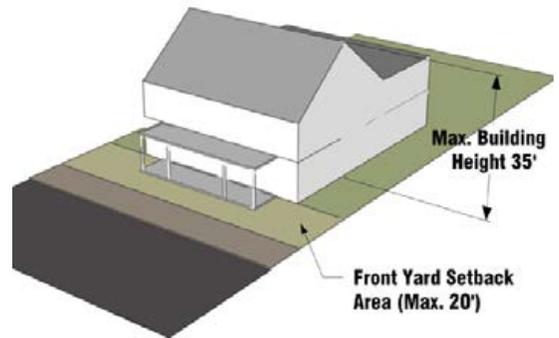
Rehabilitations

Rehabilitations, alterations, additions to, and adaptive reuse of buildings identified of historical and/or architectural interest shall respect the original design character and materials of the building. Original features shall be repaired or replaced to match. Inappropriate changes to street facing facades, such as reducing the size of window openings, eliminating window openings, removing original decorative trim, removing original porch columns, or covering original materials with vinyl siding shall be avoided.

New Construction

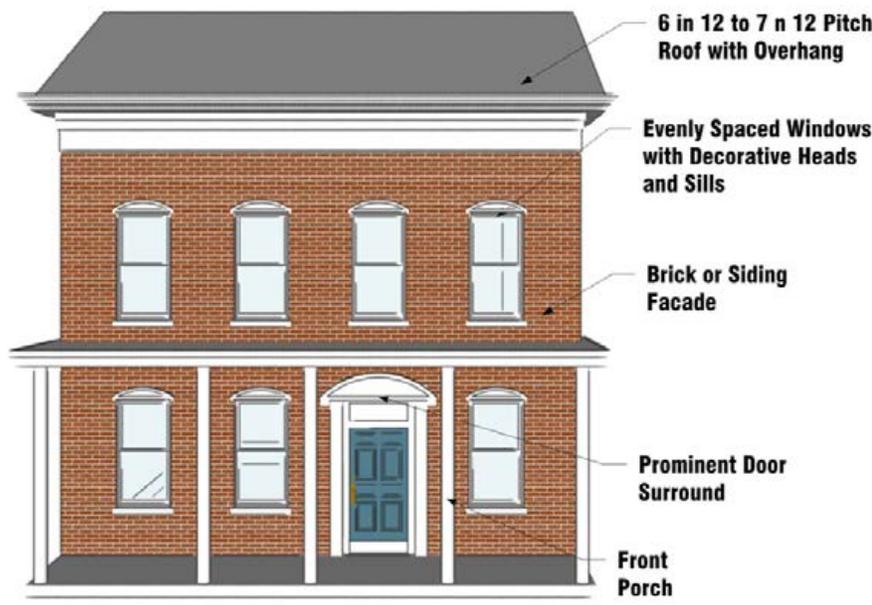
Newly constructed dwellings shall incorporate traditional features typical of the existing building fabric, including 6 in 12 to 7 in 12 pitch roofs with overhangs, brick or siding facades, evenly spaced vertical proportion windows with prominent decorative heads and sills or brick facades, and prominent decorative window trim on siding facades; porches and prominent door surrounds.

DIAGRAM 13: Front Yard and Building Height



Inappropriate window replacement and vinyl siding on Pine Street

DIAGRAM 14: New Construction Features



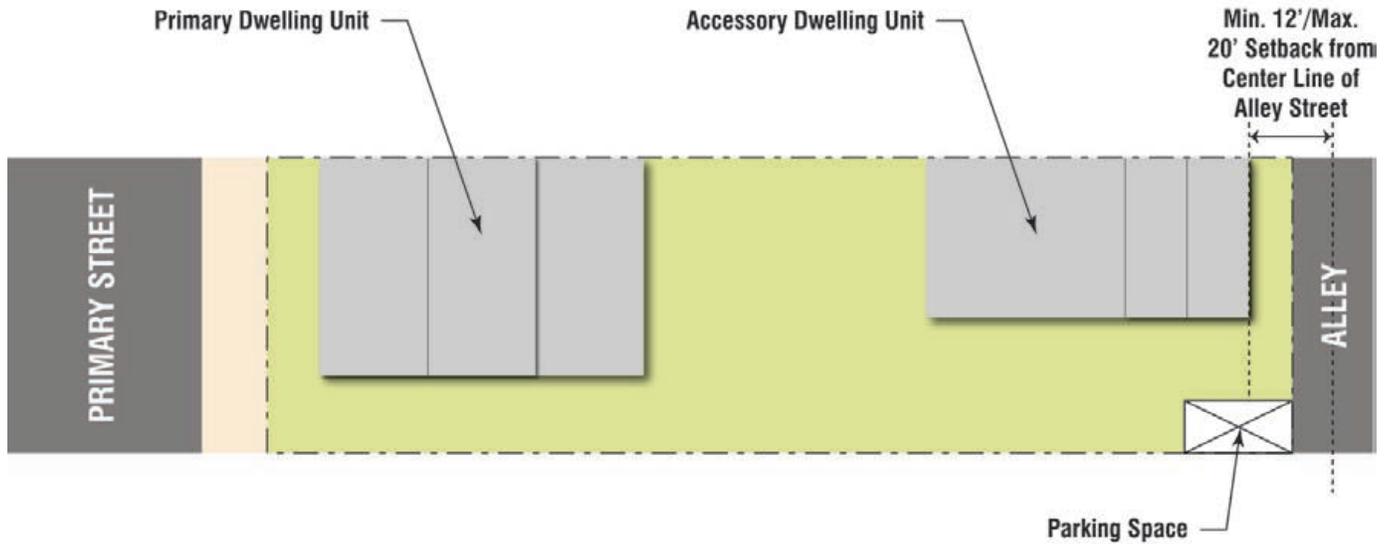


Example of an Accessory Dwelling Unit on an alley

Accessory Dwelling Units

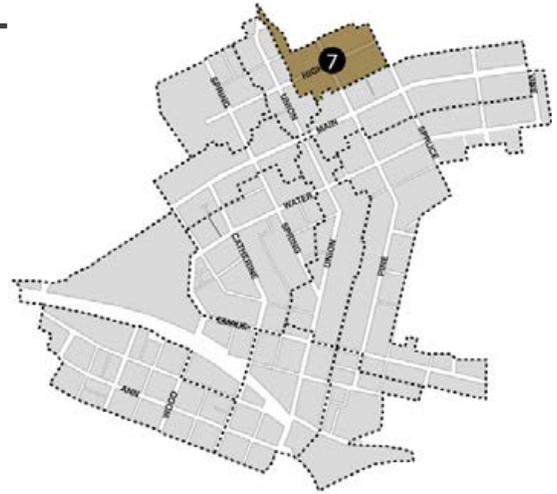
For a lot with rear access from a public alley street, an accessory single-family dwelling unit may be constructed on the same lot as the primary building. No setback is required for the accessory dwelling, but such structure including a garage structure, shall be constructed with a minimum setback of 12' from the center line of the alley street, to provide for a 24' wide vehicular accessing between building facades. Maximum setback shall be 20' from the center line of the alley street. A minimum of one parking space for each accessory dwelling unit shall be provided. Other dimensional regulations shall meet the requirements of the principle building.

DIAGRAM 15: Accessory Dwelling Unit
For Lots with Rear Access from a Public Alley Street



7

C2 ZONED AREA EAST OF NORTH UNION STREET



Existing Character

This zone includes the parcels north of Main Street and west of Spruce Street (not included in the Main Street and North Union Street Character Zones) that have predominantly commercial uses, with some limited residential uses. Commercial uses include the large school bus storage and maintenance facility at the corner of Spruce and High Street, the American Legion Post on High Street and the large warehouse/storage building at Pine and High Street. Residential uses are predominantly manufactured homes of recent construction. Historic building fabric is limited to properties fronting High Street, immediately east of Main Street.

North Union Street Development Design Issues

1. **Lack of landscaping.**
2. **Lack of quality buildings.**
3. **Need for appropriate landscaping and design treatment of commercial uses in area north of Main Street and east of Union Street.**



Bus storage facility at Spruce and High Streets



American Legion building on High Street



1



2



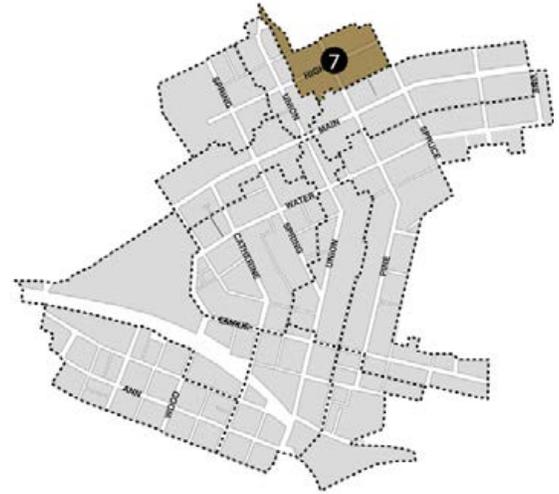
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Manufactured homes on Pine Street

Development Goals

- Encourage higher quality buildings and improved landscaping on blocks north of Main Street.
- Encourage preservation and rehabilitation of historic fabric.



C2 Zoning Overlay Regulations and Design Standards

C2 regulations design standards listed here are based on the assumption that zoning for north side of Water Street between Union and Vine Street, and parcels fronting North Pine, Spruce and Race between Main and Water Street will be changed from C2 to R2 and new commercial uses on Water Street will be prohibited, and any existing commercial uses will be non-conforming.

Uses and Dimensional Standards

TABLE 12: Zone 7 Uses, Dimensional Standards, and Minimum Parking Provisions

Uses	No changes proposed.	
Dimensional Standards	<i>Front Yard</i>	No front yard is required. Maximum front yard setback shall be 10', unless existing front yard setback for adjacent properties is larger than 10', in which case the front yard setback shall be the average of the abutting properties.
	<i>Building Height</i>	Maximum building height for all uses is thirty-five (35) feet.

Building Design Standards and Guidelines

Demolition

Demolition of buildings identified as by of architectural and/or historical interest is prohibited, unless approved by the Planning Committee, and Zoning Hearing Board.

Repairs, Alterations, and Rehabilitation of Existing Buildings

Repairs, alterations and rehabilitation of buildings identified as of historical or architectural interest, and contributing to the character of the character district and streetscape within the Zoning Overlay District shall respect the original character of the building. Original features of street facing facades of the building, including cornices, door trim, porches, porch columns, and railings shall be retained and repaired or replaced with items of a similar design and materials as appropriate. Inappropriate changes to the street facing facades of buildings, including reducing the size of or eliminating window openings, shall not be permitted.

New Construction

Exterior materials shall be high-quality materials and may include brick masonry, stone, cement stucco, and glass. Painted or unpainted concrete masonry units, or vinyl siding are prohibited.

DIAGRAM 17: Building Features to be Preserved



DIAGRAM 16: Front Yard and Building Height





Example screening of an outdoor dumpster

Streetscape

Sidewalks

Sidewalk paving shall be continuous across any driveway's curb cut. No curbed driveway access across sidewalks is permitted.

Parking Lots

Parking shall be screened from view from a public street with a combination of shrub plantings within the required 3'-0" planting strip, and installation of a screen fence of a design consistent with the traditional built fabric of Middletown.

Outdoor dumpsters and trash bins shall be screened from view on all four sides with fences, masonry walls, or landscaping. Chain link fence shall not be permitted.

Street Trees

Any new construction shall require the planting of street trees, minimum 2" caliper, at minimum every 30'.

Parking

Base zoning parking requirements under Article 16 shall be modified as shown in Table 13.

TABLE 13: Zone 7 Parking Requirements

Single and two-family detached, semi-Detached, and attached dwellings	1 space per dwelling unit
Apartment houses/multi-family	.75 space per dwelling unit
Student Housing	.75 space per dwelling unit
Motels, tourist rooms, hotels	1 space per dwelling unit

COMPLIANCE

Overlay Districts

If the provisions of an overlay district conflict with other provisions of this zoning code the overlay provisions shall govern, regardless of whether they are more restrictive or less restrictive than the conflicting provision.

Overlay zoning district regulations apply in combination with underlying base zoning district regulations and all other applicable regulations of the zoning code. All applicable zoning code regulations apply in overlay districts, except that when overlay district standards conflict with standards that would otherwise apply under this code, the regulations of the overlay district governs.

Article 20 of the Zoning Code, Administration and Enforcement, would include a new Section G under 260-2006 Application for Zoning Permits. In addition to the submission requirements listed under 260-2006, Applications for Zoning Permits with the Revitalization Overlay Zone shall also submit the following:

Compliance: Review and enforcement of the design standards will require that the applicant submit sufficient documentation to illustrate how the development, whether new construction or alterations, will comply with the development design standards and architectural design standards.

Applicability

Any new or expanded building, or any building that is rehabilitated, where such rehabilitation is equal to or exceeds 50% of the existing floor area of the building, or includes demolition and/or repair or rehabilitation of the street facing facades of the building, shall comply with the development design standards in Subsection ____.

Compliance

The applicant shall submit architectural drawings in conjunction with a development or permit application that demonstrate how the development will comply with the development design standards and architectural design standards. The documentation shall include a list of proposed exterior materials, and two (2) copies of the following items:

Site plan, drawn to scale, annotated to show compliance with the development standards,

Proposed street facing building elevations and adjacent building context, drawn to scale, annotated to show compliance with the design standards, including materials. For repair or rehabilitation of street facing facades of a building, applicant may submit annotated photos of the facades showing proposed work and materials.

For new construction or addition, a colored rendering, annotated to show compliance with design standards, landscape plan, etc.

For new construction, current photographs of site and adjacent buildings. For rehabilitation, current photographs of building and adjacent buildings. For a proposed demolition, current photographs of building and adjacent buildings.

